



**NEW  
PARRAMATTA  
RD**

**Consultation Outcomes Report  
Draft Parramatta Road  
Urban Transformation Strategy**

**MAY 2016**



The draft Strategy is the NSW Government's 30-year plan for the Parramatta Road Corridor that will see growth and renewal focused in well-established areas close to existing public transport, infrastructure, jobs and services.



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# 1



## About this report



# Introduction

The NSW Government's [A Plan for Growing Sydney](#) estimates that Sydney's population will grow by 1.6 million people over the next 20 years. The Plan nominates the Parramatta Road Corridor (the Corridor) as a major area for increased housing, economic activity and social infrastructure, especially around centres with good public transport access and amenity.

There is wide agreement in the community that the Corridor is not living up to its potential. People want less traffic, more attractive public spaces and better public transport.

The [draft Parramatta Road Urban Transformation Strategy](#) (the draft Strategy) was developed to respond to two broad imperatives: to support Sydney's growth and to make the Corridor a better place to live, work and visit. The draft Strategy's vision for the Corridor is:

❖ A high quality multi-use corridor with improved transport choices, better amenity and balanced growth of housing and jobs'.

UrbanGrowth NSW has been working closely with local councils and other state government agencies to develop and seek feedback on the draft Strategy. Collaboration across state and local governments is vital for ensuring growth and renewal is coordinated along the Corridor's 10 local government areas.

To achieve the best outcomes for the Corridor, we also need to understand the needs and interests of the community, particularly the people who will be directly impacted by the proposed changes.

Consultation with the community began in late 2013 and in late 2014 the draft preliminary Strategy was displayed for comment. We used community feedback to update the draft Strategy, which was released in September 2015.

The consultation program for the updated draft Strategy took place between October and December 2015. Over this time, we spoke with hundreds of people who care about the future of the Corridor. While feedback varied from Precinct to Precinct, there was broad consensus from councils and communities that:

- development needs to be sensitive to local character, and
- population growth needs to be supported by public transport, social infrastructure, open space and amenity improvements.

There were a number of other key themes that emerged from the consultation program: traffic, walking and cycling, parking, property impacts, and the consultation program itself. These findings are presented in detail throughout this report.

We want to thank everyone who took the time to share their views on the draft Strategy. We will work closely with local councils and other state government agencies to use this feedback to finalise the Strategy in the first half of 2016.

## How this report is structured

This report presents the findings from public consultation on the draft Strategy between September and December 2015. This report is structured as follows:

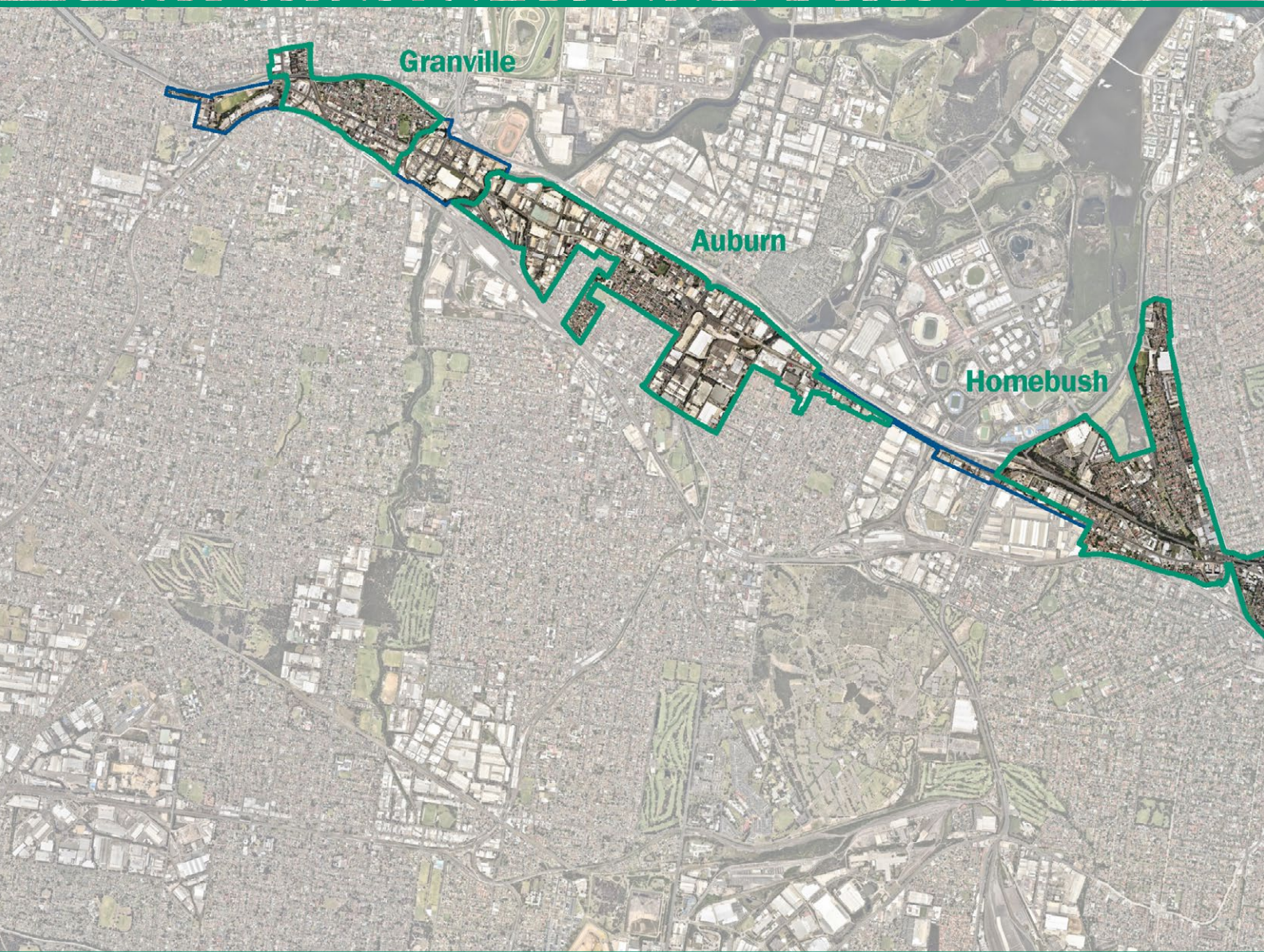
- a summary of the key themes raised in the feedback and our initial response to this feedback (chapter 2)
- a summary of feedback received on each of the eight Precinct Plans and our initial response to this feedback (chapter 3)
- community consultation methodology (chapter 4), and
- next steps for finalising the Strategy (chapter 5).

Detailed research findings are located in the Appendices.

## The Parramatta Road Corridor study area

The Parramatta Road Corridor encompasses the land adjoining and at least one block back from Parramatta Road, as well as eight Precincts that are areas of proposed growth. The Corridor runs for about 20 kilometres, connecting Sydney's two CBDs: Parramatta in the west and Sydney CBD in the east.

# Parramatta Road Corridor



 Precinct Area  Frame Area





















**Eight Precincts** have been identified by UrbanGrowth NSW, in conjunction with local councils, to be the focus of growth and renewal within the Corridor because of their access to jobs, transport, infrastructure and services.

**The Frame Areas** are the stretches of land that front Parramatta Road and run between each Precinct. They typically include the lots facing Parramatta Road and the first street running parallel to Parramatta Road.



## Consultation snapshot

The people who live, work and visit the Corridor come from a range of socio-economic groups, all age groups, and from culturally and linguistically diverse backgrounds. The table below shows the range of events, information and feedback channels we used to cater to these different groups' needs, preferences and circumstances. In total, we received over 3,700 responses to the draft Strategy through submissions and the online/paper survey. For more detail on the consultation methodology and the consultation tools used, please see chapter 4.

Communicating about the consultation process			
 <p>Letterbox drop to <b>105,000</b> homes</p>	 <p>Letters to <b>22,000</b> property owners</p>	 <p>Advertisements in <b>7</b> English language newspapers</p>	 <p>Advertisements in <b>12</b> foreign language newspapers</p>
Community engagement techniques and participation			
 <p><b>23</b> Pop up stalls at shopping centres, markets and train stations</p>	 <p><b>10</b> Focus groups</p>	 <p><b>10</b> Information and feedback sessions</p>	 <p>Phone survey of <b>518</b> residents</p>
 <p><b>339</b> Calls to 1300 line</p>	 <p><b>696</b> Project emails received</p>	 <p><b>21,298</b> Website visits</p>	 <p><b>4</b> Mayor &amp; MP Forums, plus MP mobile office</p>
 <p><b>59</b> Facebook updates</p>	 <p><b>9</b> Councillor briefings</p>	 <p><b>3</b> Town Hall meetings</p>	 <p><b>8</b> Industry &amp; Chamber of Commerce meetings</p>
 <p><b>15</b> Landowner &amp; developer meetings</p>	 <p><b>10</b> Interpreters provided across different consultation events</p>	 <p><b>939</b> Total number of submissions received</p>	 <p><b>2,779</b> Responses to the paper and online survey</p>





# 2



## Feedback summary



# About this chapter

## This chapter presents:

- a summary of the comments most frequently made by the community and local councils about the proposed changes to the Parramatta Road Corridor and the eight Precincts, and
- our initial response to this feedback, explaining how it will be used to finalise the draft Strategy.

To see council and community feedback on the individual Precinct Plans, please refer to chapter 3. Detailed research findings are located in the Appendices.

## Feedback summary

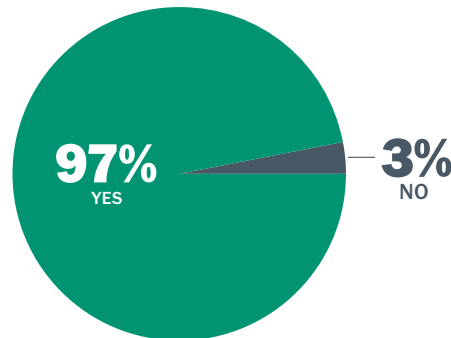
To get feedback on the draft Strategy, we needed to understand the community and local councils' responses to these four broad questions:

- Is there agreement that the Corridor needs to be revitalised?
- If so, what needs to be improved?
- How well does the draft Strategy address the Corridor's needs?
- What do we need to consider when finalising the Strategy?

To get answers to these questions we sought council and community feedback using a random telephone survey, written submissions and an online/paper survey. An overview of the feedback we received is presented below.

### Does the Corridor need to be revitalised?

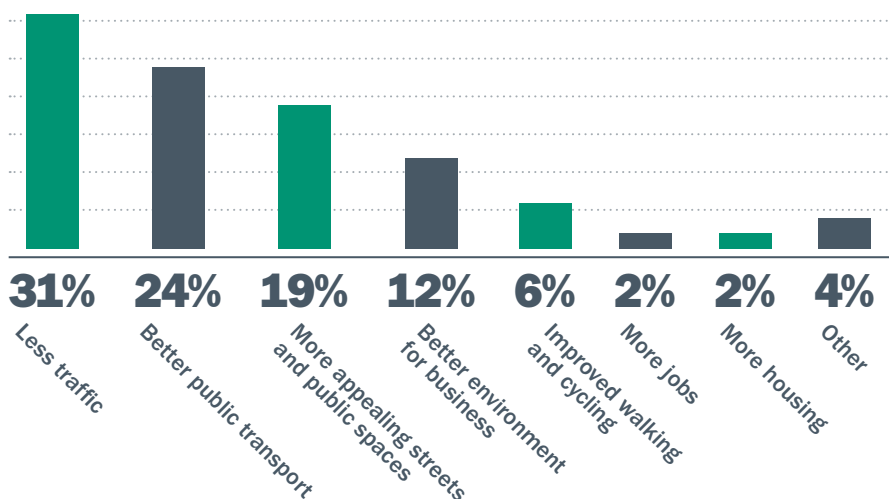
We gauged the community's views on the need to revitalise the Corridor using a random telephone survey that was statistically representative of communities along the Corridor. As shown in the pie chart adjacent the vast majority of interviewees (97%) told us that the Corridor was in need of revitalisation.



### What needs to be improved along the Corridor?

As shown in the graph below, people thought the Corridor was most in need of less traffic, better public transport and more appealing streets and public spaces.

#### What the Corridor needs

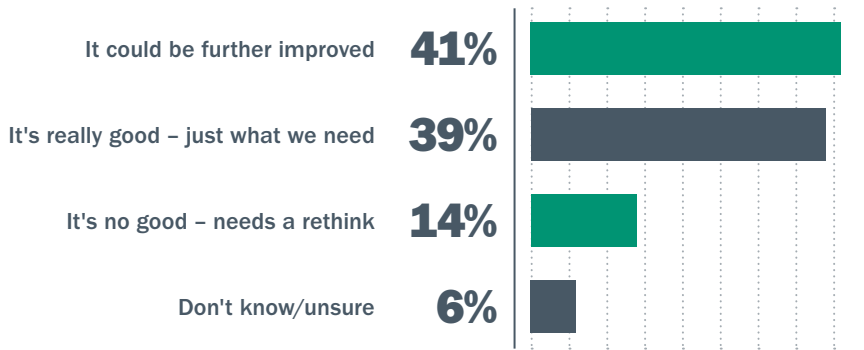


Source Random telephone survey

### How well does the draft Strategy address the Corridor's needs?

We asked interviewees how well they thought the draft Strategy addressed the elements identified as needing improvement. As shown in the graph below, the responses were fairly evenly split between those who thought the draft Strategy was 'really good' and those who thought it 'could be further improved.'

How people felt about the draft Strategy

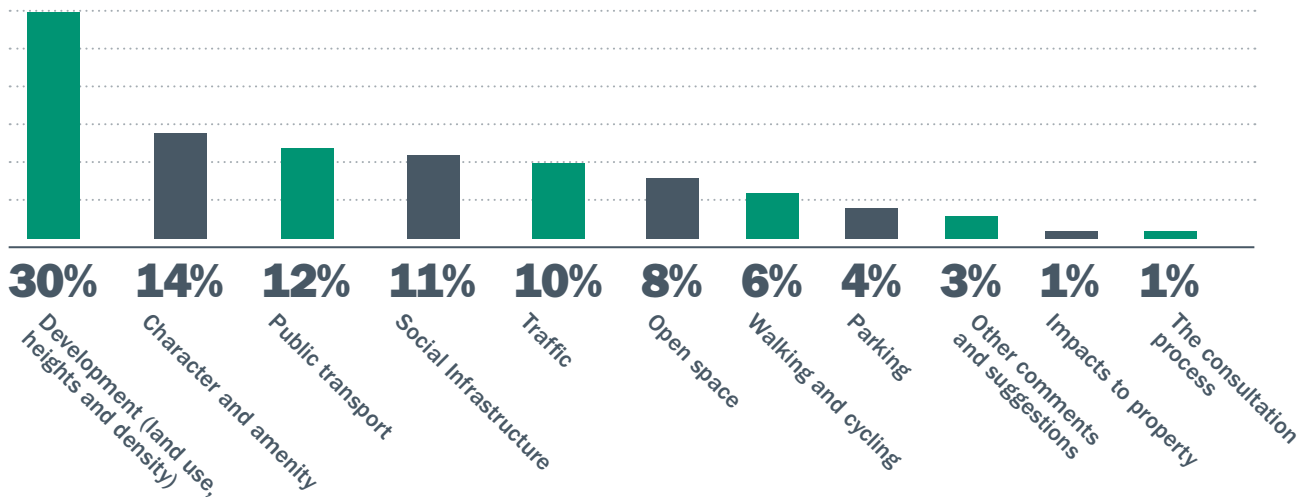


Source Random telephone survey

### What do we need to consider when finalising the Strategy?

To better understand people's responses to the draft Strategy, we invited the community and local councils to share their detailed feedback through written submissions and by responding to the online/paper survey. We categorised their feedback under 11 themes that reflect the comments we heard most consistently. As shown in the graph below, we received the most comments on the themes of development, character and amenity, public transport, traffic, social infrastructure and open space. In the following section, we provide a summary of the comments we heard under each of these themes.

Most frequently raised themes



Source Online/paper survey and written submissions

## Council and community feedback on key themes

The summary of council and community feedback below reflects the topics most frequently raised during consultation. Where appropriate, we have complemented this feedback with the results from the random telephone survey. Initial responses have been made to the topics raised, indicating how we will use this information to finalise the Strategy. To see council and community feedback on the individual Precinct Plans, please refer to chapter 3.

### Development

Development was the most frequently discussed theme in council and community feedback, accounting for 30% of comments made in submissions and online/paper surveys.

While the feedback suggests that the majority of the community is, in principle, open to the idea of more development in the Corridor\*, it also shows that people have a number of concerns with the development proposed in the draft Strategy.

**The most commonly raised concerns relate to the locations of tall and high-density development in existing residential areas and impacts on heritage and the local character.**

Other comments frequently made in the feedback included:

- recommendations for concentrating development along or near Parramatta Road, away from existing residential and heritage areas
- concerns about overshadowing and loss of privacy
- recommendations for better integrating developments by increasing setbacks and stepping heights
- strong interest for developments to include provisions for affordable housing
- request for more detail about the staging of proposed developments
- request for more detailed information on what is proposed for the Frame Areas
- support for business and employment opportunities through proposed land uses
- concerns about the role of private developers in implementing the Strategy
- strong interest for design principles to guide new development, and
- also we received alternate proposals for Taverners Hill from Marrickville and Leichhardt Municipal Councils.

### Our initial response

As increased development for housing and employment is fundamental to the revitalisation of the Corridor to support Sydney's growth, we will continue to plan for increased heights in the eight Precincts. However, to address the specific council and community concerns about the proposed development, we will:

- investigate the potential redistribution of land uses in some areas and amendments to scale where appropriate
- update the Urban Design Guidelines to propose strategic locations adjacent to public transport that are best suited for increased heights, while also having the least impact on adjoining areas
- further investigate the impacts of shadowing and privacy on surrounding areas from new development
- complete additional urban design work to more clearly define building heights and identify areas where transitions are required
- develop an affordable housing target in collaboration with government agencies and local councils that meets the needs of the future population
- continue to investigate, in collaboration with government agencies and local councils, how affordable housing options can be delivered, and
- consider elements of alternate proposals in the refinement of the final strategy.

Further community consultation will occur prior to any proposed changes to local zoning and development controls. These changes will be facilitated and approved by the NSW Department of Planning and Environment (DPE), the Greater Sydney Commission (GSC) or the local councils. Rezoning will only occur after the adoption of the final Strategy and is intended to be implemented in stages in response to the delivery of key infrastructure.

\* 61% of phone survey interviewees disagreed or strongly disagreed that 'there was no need for more development in the Parramatta Road Corridor area'.

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## Character and amenity

Character and amenity accounted for 14% of comments made in surveys and submissions. This broadly aligns with telephone survey results, which showed that ‘more appealing streets and public spaces’ ranked as the Corridor’s third highest need. In the telephone survey, 19% of interviewees nominated this as the Corridor’s highest need.

**The feedback was largely supportive of the Urban Amenity Improvement Program as a first step to creating more attractive public spaces along the Corridor.** Other comments frequently made included:

- strong support for more trees/greenery
- requests for more funding for the Urban Amenity Improvement Program
- concern about the impact of new development on the character and heritage of existing homes and buildings, and
- concern about air and noise quality.

### **Our initial response**

To address concerns about the draft Strategy’s impact on the character and heritage of neighbourhoods, we will work with councils to undertake further heritage analysis within the Precincts and Frame Areas. We will also investigate additional planning measures that will further protect or better respond to areas with established character and low scale.

To support this work, we have engaged a heritage consultant and architect to work with our urban designer to:

- map all existing heritage items and heritage conservation areas across the entire Corridor for inclusion in the final Strategy
- recommend additional properties and landscapes that could be considered for heritage listing or protection, and
- review and refine heritage controls for inclusion in the Urban Design Guidelines.

Also we intend to incorporate measures into the Urban Design Guidelines to address air and noise quality.

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## Public transport

The feedback shows that improving transport in the Corridor is a priority for the community. Public transport accounted for 12% of comments made in surveys and submissions. The importance of public transport was reinforced by telephone survey findings, which showed that ‘better public transport’ was ranked as the Corridor’s second highest need after less traffic.

**The most common concern raised in feedback was the need for the growth of the Corridor to be supported by significant improvements to public transport services.** Other comments frequently made on public transport included:

- requests for more detail about how the NSW Government will fund and deliver public transport improvements, and
- calls for the better integration, frequency and reliability of existing transport services.

### **Our initial response**

All council and community feedback about transport services will be provided to Transport for NSW (TfNSW) and Roads and Maritime Services (RMS) to inform their public transport and roads planning along the Corridor.

Rezoning of land will be triggered by mechanisms that align the coordination and delivery of services and infrastructure as identified by DPE and GSC.

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## Social infrastructure

Social infrastructure accounted for 11% of comments made in surveys and submissions.

**The feedback centred on concerns that the draft Strategy did not include clear commitments to the additional social infrastructure needed to support population growth.** Schools and health services were the most frequently cited social infrastructure needed in the Corridor.

### Our initial response

To address council and community concerns about the need for social infrastructure to support the growth and revitalisation of the Corridor, we will:

- continue to work with local councils and government agencies such as the NSW Department of Education (DOE) and NSW Health to plan for the social infrastructure to support growth
- collaborate with relevant government agencies to develop funding and delivery timeframes, and
- ensure future social infrastructure planning focuses on safety and convenience.

Rezoning of land will be triggered by mechanisms that align the coordination and delivery of services and infrastructure as identified by DPE and GSC.

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## Traffic

Traffic accounted for 10% of comments made in surveys and submissions. **People were most commonly concerned with projected population growth compounding existing traffic congestion.** These concerns are supported by the results from the telephone survey, which showed 'less traffic' ranked highest in the Corridor's needs (31% of interviewees ranked this as the Corridor's highest need).

Other comments included:

- concerns that WestConnex will increase traffic along Parramatta Road, and
- support for car-sharing programs and other initiatives to reduce car travel within the Corridor.

### Our initial response

To address concerns about traffic congestion, we will work with TfNSW, RMS and local councils to:

- conduct analysis to support planning for each Precinct to manage local traffic and improve pedestrian amenity as part of future rezoning
- identify intersection upgrades, road improvements, and to assess the need for changing speed limits, and
- analyse north-south crossings across Parramatta Road in more detail as the planning for the Precincts takes shape and as the Bus Rapid Transit and light rail designs are progressed.



## Open space

Open space accounted for 8% of comments made in surveys and submissions. There was broad consensus in the feedback that **open space was critical for supporting population growth along the Corridor, with many people commenting that the draft Strategy's provisions for open space were inadequate.** Other comments frequently made in the feedback included requests for more detailed information about who would deliver and fund the open space in each of the Precincts.

### Our initial response

To address concerns about the open space in each of the Precincts, we will:

- revisit the locations, amount and type of proposed open space in each precinct in consultation with the NSW Government Architect's Office, local councils and other stakeholders
- set out the general process and anticipated delivery timing of any new open space or new connections located over private property
- review proposed vehicular connections to open space, and
- collaborate with government stakeholders to nominate the relevant authority for delivering open space.



## Walking and cycling

Walking and cycling accounted for 6% of comments made in surveys and submissions. **Comments made on this theme were largely supportive of the proposed improvements to walking and cycling routes within the Corridor, with safety being raised as a high priority.** Other comments included interest in making streets and buildings more accessible.

### Our initial response

To strengthen the proposed improvements to cycling and walking options along the Corridor, we will:

- finalise a strategy that supports increasing focus on cycling and walking
- work with other government agencies to ensure social infrastructure planning includes a focus on active transport, and
- provide more emphasis on the amenity of streets, making walking and cycling to local places, services and activities enjoyable and safe.



## Parking

Parking accounted for 4% of comments made in surveys and submissions. **Comments on this topic centred on concerns that the growth of the Corridor will compound existing parking shortages experienced by residents, visitors and local business.**

### Our initial response

To ensure there is enough parking in the Precincts to cater for residents, visitors, and businesses, we will:

- further develop parking management strategies
- investigate demand management actions aimed at supporting changing travel behaviours, including the role of parking to encourage active transport and promotion of car share schemes, and
- explore other opportunities to reduce car usage.

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## Other

Feedback that could not be accommodated into other listed themes accounted for 3% of comments made in surveys and submissions. Frequently raised topics included:

- requests for **more funding and implementation detail**
- concerns about **flood management**, and
- suggestions that the Strategy directly addresses **community health and wellbeing** along the Corridor.

### **Our initial response**

We will consider these comments in the refinement and finalisation of the Strategy.

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## Impacts to Property

Feedback on property impacts accounted for 1% of comments made in surveys and submissions. The comments largely focused on **concerns about compulsory acquisitions and reductions in property values**.

### **Our initial response**

Rezoning of land will be triggered by mechanisms that align the coordination and delivery of services and infrastructure as identified by DPE and GSC.

In the event that a compulsory land acquisition is needed to deliver infrastructure or open space, this will occur in full consultation with the landowner. UrbanGrowth NSW does not have the power to make compulsory acquisitions. Any land acquisitions would be undertaken by the state or local government agency responsible for delivering the infrastructure or open space.

We strongly recommend that landowners obtain independent professional advice prior to making any significant decisions about their property.

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## Consultation

Feedback on the consultation program accounted for 1% of comments made in surveys and submissions. **Comments made on this theme most frequently focused on requests for more detailed information about aspects of the draft Strategy.** Other comments included requests for further community consultation prior to the finalisation of the Strategy.

### **Our initial response**

UrbanGrowth NSW will use this feedback to inform future communication and consultation.

Further community consultation will occur prior to any proposed changes to local zoning and development controls. These changes will be facilitated and approved by DPE, GSC or the local councils. Rezoning will only occur after the adoption of the final Strategy and is intended to be implemented in stages in response to the delivery of key infrastructure.





# 3

## Feedback on the Precincts

# About this chapter

This chapter provides a summary of the feedback on the eight Precinct Plans, including:

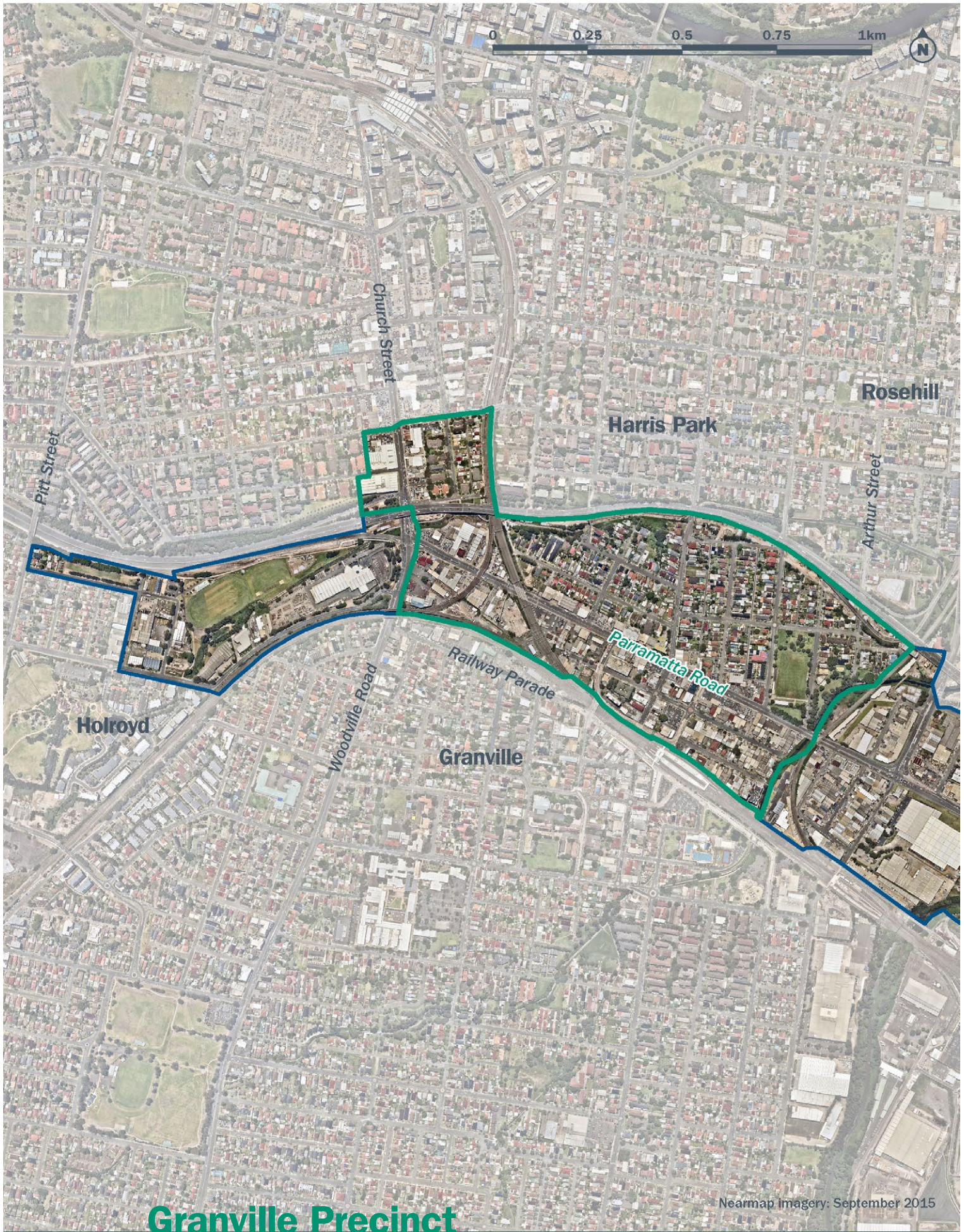
- a summary of community feedback on each Precinct Plan
- a summary of local council feedback on each Precinct Plan, and
- our initial response to community and local feedback on each Precinct Plan.


## About the Precincts

The draft Strategy identified eight Precincts to be the focus of growth and renewal within the Corridor because of their access to jobs, transport, infrastructure and services. Each Precinct is being planned to accommodate a different mix of housing, jobs and public spaces in a way that is sensitive to its character and heritage.

For more information on the eight Precinct Plans, please see the draft Strategy at [www.urbangrowth.nsw.gov.au](http://www.urbangrowth.nsw.gov.au)





 Precinct Area     Frame Area

# Feedback on the Granville Precinct Plan

**Vision:** Close to Sydney’s second CBD at Parramatta, Granville will be a vibrant mix of new housing, shops and commercial spaces, linked by a much improved network of streets and attractive new parks and public spaces.

**Source** [draft Parramatta Road Urban Transformation Strategy](#), September 2015. All vision statements were developed with councils.

## Engagement snapshot for the Granville Precinct

The following table outlines the diverse range of feedback channels and consultation activities^ that were organised for the Granville Precinct.

				ACTIVITIES
 <b>2</b> shopping centre displays	 <b>1</b> community market pop up	 <b>2</b> train station pop ups	 <b>1</b> community information and feedback sessions	
				RESPONSE CHANNELS
 <b>49*</b> responses to the paper and online surveys	 <b>7</b> focus group participants	 <b>62</b> community submissions	 <b>2</b> council submissions	

\* The total number of people who submitted a survey is greater than the number above, as not all participants linked their survey to a Precinct.

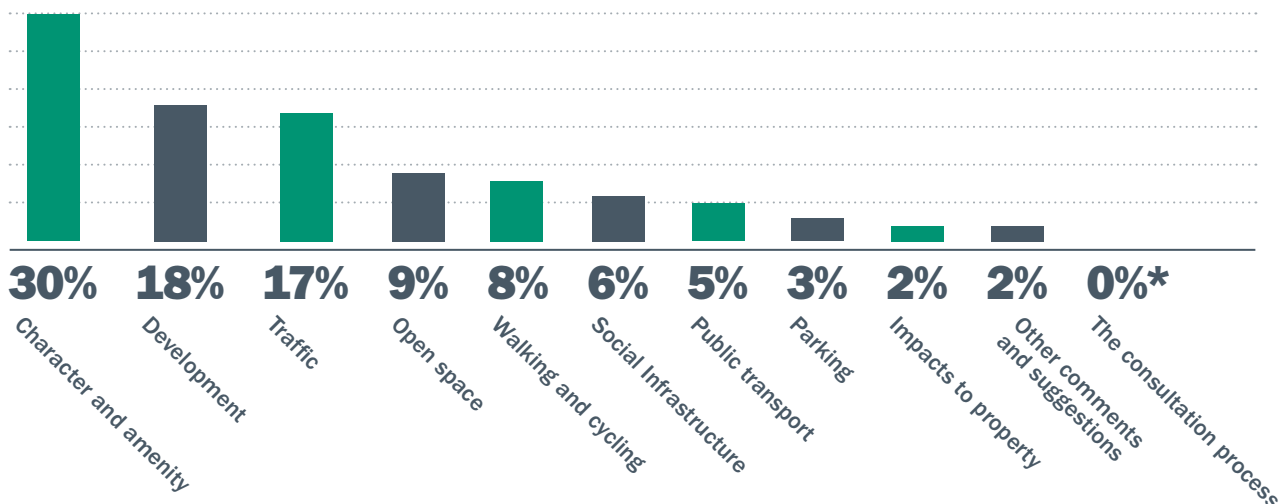
^ All activities were organised on an LGA basis.



To see more details on the Granville Precinct Plan please see the draft Parramatta Road Urban Transformation Strategy at [www.urbangrowth.nsw.gov.au](http://www.urbangrowth.nsw.gov.au)

## Feedback on key themes

The graph below presents council and community feedback on the Granville Precinct Plan against 11 themes. The data draws from online/paper surveys and submissions. A breakdown of feedback from councils and the communities is provided in the following sections.



The percentages in the above graph indicate the frequency of comments made by key theme. \* represents less than 0.5% of responses

## What we heard from councils

Submissions on the Granville Precinct Plan were made by Holroyd City Council and Parramatta City Council. A summary of their comments and recommendations is provided below.

### Holroyd City Council

- Support for the inclusion of the Mort Street industrial area and the proposed land use zoning and maximum building heights
- Request for clarity on the role of Frame Areas, including the level of development allowed in these areas
- Request for clarity about the statutory mechanisms proposed and the timing of the implementation program
- Support for density on the basis that it is tied to adequate open space, community assets, and healthy environments
- Request for the Crescent Street and Woodville Road intersection within the Granville Precinct to be shown on the Precinct Plans, as well as opportunities to provide future connecting routes on Crescent Street to the Merrylands Centre and the Holroyd Frame Area
- Recommend the draft Strategy address potential health impacts from reductions in air and noise quality associated with increased density
- Concern that the Urban Amenity Improvement Program budget of \$200 million will fall short of the actual needs of the Corridor.

## Parramatta City Council

- Support for heights greater than the current 52 metres where the development is on a site over 3,200m<sup>2</sup>
- Support for the transition of the existing B6 Enterprise Corridor land along Parramatta Road to a mixed use zone as proposed in the draft Strategy
- Support for the draft Strategy's nomination of Enterprise and Business land use along the Church Street frontages
- Note that the draft Strategy does not allow for the provision of affordable housing within the Granville Precinct
- Concern the significant difference in heights will result in poor transitioning
- Request for variety in built form and amenity of new buildings
- Recommend that the upper setback follow the Council's Development Control Plan (DCP) of a minimum of six metres, with smaller upper setback towers creating poor wind effects on the ground
- Support for front setbacks for new residential development to be between five and nine metres
- Recommend the Parramatta Ways (Green Grid) links for Granville Precinct be incorporated into any future precinct DCP for the site
- Recommend the reconfiguration and upgrade of FS Garside Park
- Oppose the revegetation or reconfiguration of the existing sports field in the short to medium term
- Note the area between Duck Creek and the Carlingford railway line (1c Arthur Street) is not open space, as the area is owned by RMS and is inaccessible to the public
- Concern that WestConnex will not reduce congestion on Parramatta Road
- Recommend the NSW Government fund the investigation of intersection works at Cowper and Bold streets, with RMS and Council to form a steering group to guide implementation
- Request the NSW Government be responsible for delivering the extension of Alfred Street between Parramatta Road and Cowper Street
- Request an alternative funding source be allocated towards the Bold Street extension and the addition of a council funded small road widening reservation and short left turn lane for southbound traffic along Good Street at Parramatta Road
- Recommend significant increases to the service frequency (both east and west) at Granville Railway Station
- Recommend that bus priority measures do not involve a dedicated bus lane along Parramatta Road, west of Auburn
- Oppose underground commuter car parking as part of the new urban plaza
- Recommend the designated north-south bicycle route run along Alfred Street instead of Good Street
- Recommend an additional active transport link from the end of Albert Street to the regional cycleway
- Support the inclusion of traffic lights at Alfred Street as it will facilitate a much needed crossing point for pedestrians at the eastern end of the Granville Precinct
- Request more schools be built in Granville to support population increases
- Support alternative funding for additional community facilities to meet the demands from population growth in the Precinct

## What we heard from the community

The summary of community feedback on the Granville Precinct Plan below is presented in order of the themes that received the most comments in the online/paper survey and submissions (as shown in the graph on page 19). This summary describes the most frequently raised topics under each theme – it is not exhaustive of all comments made on the Granville Precinct Plan.



### Character and amenity

Character and amenity was the most frequently discussed theme in the feedback, accounting for 30% of comments made in surveys and submissions. Frequent comments included:

- **support for public domain improvements, such as landscaping, streetscaping, more public toilets and gathering spaces,** and
- **requests for a larger share of the \$200 million Urban Amenity Improvement Program.**

📌 The provision of green spaces where residents can walk, cycle, participate in active and passive recreation and experience a sense of community by interaction with neighbours is key to implementing liveable and sustainable communities’.  
– *community feedback*



### Development

Development was the second most frequently discussed theme in the feedback, accounting for 18% of comments made in surveys and submissions. **Community feedback on the proposed development for the Granville Precinct was divided between those who supported and opposed the changes.** Other comments included:

- concern about impacts on residential areas
- interest in ensuring new developments are guided by quality of design
- concern about developments creating ‘street canyons’
- recommendations for setbacks to between five and nine metres, and
- suggestions for spreading taller buildings across the Precinct with stepped height.

📌 The significantly higher density of residential properties is not matched by a commensurate density of green space’.  
– *survey response*

## Traffic

Traffic accounted for 17% of comments in surveys and submissions, with **most people raising concerns about increased population compounding existing congestion.** Other comments included:

- identification of traffic congestion on key problem intersections including Church Street/Parramatta Road/Woodville Road/M4 westbound on ramp intersection; Bold Street/Parramatta Road; and Good Street/Parramatta Road
- suggestions to widen Parramatta Road
- concerns that WestConnex will not reduce congestion on Parramatta Road, and
- concerns that development along Parramatta Road will generate heavy traffic.

## Open space

Open space accounted for 9% of comments made in surveys and submissions. **Most of the comments expressed the need for more open space in the Granville Precinct.** Other comments included:

- suggestions for the inclusion of both passive and active recreation areas, and
- suggestions for making the area greener to help counter heat in summer, including shady street trees and roof gardens in apartment buildings.

## Walking and cycling

Walking and cycling accounted for 8% of comments in surveys and submissions, with **the majority of comments welcoming the proposed improvements.** Other comments included:

- suggestions for separate active transport links from roads to improve safety, and
- requests for additional new active transport links to connect from the end of Albert Street to the regional cycleway.

“There is an existing cycleway under the [M4] motorway and a series of potential open space areas. This can be activated through a number of initiatives to effectively utilise the available space and contribute to the amount of available open space within the Granville Precinct’.  
– *community feedback*

## Social infrastructure

Social infrastructure accounted for 6% of comments in surveys and submissions. **Most of the comments centred on the existing infrastructure not being sufficient to support forecast population growth.** Other comments included:

- suggestions for the provision of additional schools and accessible public facilities, and
- suggestions for more community infrastructure to support higher density development, such as community gardens, children’s play areas and BBQs.





## Public transport

Public transport accounted for 5% of comments in surveys and submissions. **Most comments focused on the need to encourage more people to use public transport to reduce traffic in the area.** Other comments included:

- requests for more frequent rail services
- requests for improved integration of transport services
- recommendations to improve public transport services for the southern end of the Precinct, and
- concerns about the lack of detail on the expected demand for the proposed Parramatta Road bus route from Burwood to Parramatta.



## Parking

Parking accounted for 3% of comments in surveys and submissions. The majority of comments raised the need for additional general parking and commuter parking in the Precinct.



## Impacts to property

Impacts to property accounted for 2% of comments in surveys and submissions. Most comments were from landowners concerned about compulsory acquisitions and reductions in local property values.



## Other

Feedback that could not be accommodated into the other themes accounted for 2% of comments made in surveys and submissions. Comments included:

- suggestions to improve accessibility to public facilities, and
- suggestions for street safety improvements, including the provision of CCTV cameras, crime prevention through environmental design principles, and increased police patrols.



## Consultation process

Feedback on the consultation program accounted for less than 0.5% of comments in surveys and submissions, with a small number of people stating that more community consultation on the draft Strategy was needed.

## Our initial response

Further community consultation will occur prior to any proposed changes to local zoning and development controls. This consultation will occur in line with statutory processes. Where appropriate, key components of the Strategy will be included within the District Plans presently being developed by Greater Sydney Commission (GSC), Department of Planning and Environment (DPE) and councils. The District Plans will then be implemented through councils' Local Environmental Plans (LEPs) in coordination with the delivery of key infrastructure.

Below we have provided our initial responses to the matters most frequently raised in the feedback and outline how we will finalise the Strategy in collaboration with local councils and government agencies.



### Development

- We have engaged a consultant team to review the land uses, scale and access and movement principles to accommodate the projected population growth, and better understand how the proposed controls may alter Granville's existing character. Land south of Parramatta Road, Good Street and the Tottenham Street area will be areas of particular focus.
- The draft Urban Design Guidelines are being reviewed and updated in conjunction with revisions to the Precinct Plans. This will guide the quality and amenity of buildings, and address potential for overshadowing and other undesirable outcomes.
- We are working closely with government agencies, such as DPE and the Department of Premier and Cabinet (DPC) to finalise the NSW Government's position on housing diversity and affordability policy and ensure the Strategy is consistent with that broader Government policy.



### Public and active transport and traffic

- All council and community feedback about transport services will be provided to Transport for NSW (TfNSW) and Roads and Maritime Services (RMS) to inform their public transport and roads planning along the Corridor.
- Rezoning of land will be triggered by mechanisms that align the coordination and delivery of services and infrastructure as identified by DPE and GSC.
- Traffic implications of WestConnex, M4 Motorway and other urban renewal projects will be investigated in consultation with TfNSW, RMS, DPE and councils.
- We will investigate initiatives to support renewal of the Granville Precinct, including better cycling and walking routes, intersection upgrades, road improvements and potential changes to speed limits.
- Intersection upgrades at Good Street and Woodville Road will be analysed in more detail.



### Character and amenity

We have engaged a heritage consultant and architect to provide further advice on heritage items, streetscape and heritage conservation areas. Working with our urban designer, the consultant team will:

- map all existing heritage items and heritage conservation areas across the entire Corridor, including the Granville Precinct and Frame Area, for inclusion in the final Strategy
- recommend additional properties and landscapes that could be considered for heritage listing or protection, and
- review and refine heritage controls for inclusion in the Urban Design Guidelines, with particular emphasis on heritage floor space transfers, minimum setbacks, integration and transition principles.

The architect will also prepare a series of indicative floor plates that demonstrate best practice when designing residential, retail and commercial uses on busy roads such as Parramatta Road. It is intended that these floor plates would form part of the revised Urban Design Guidelines.



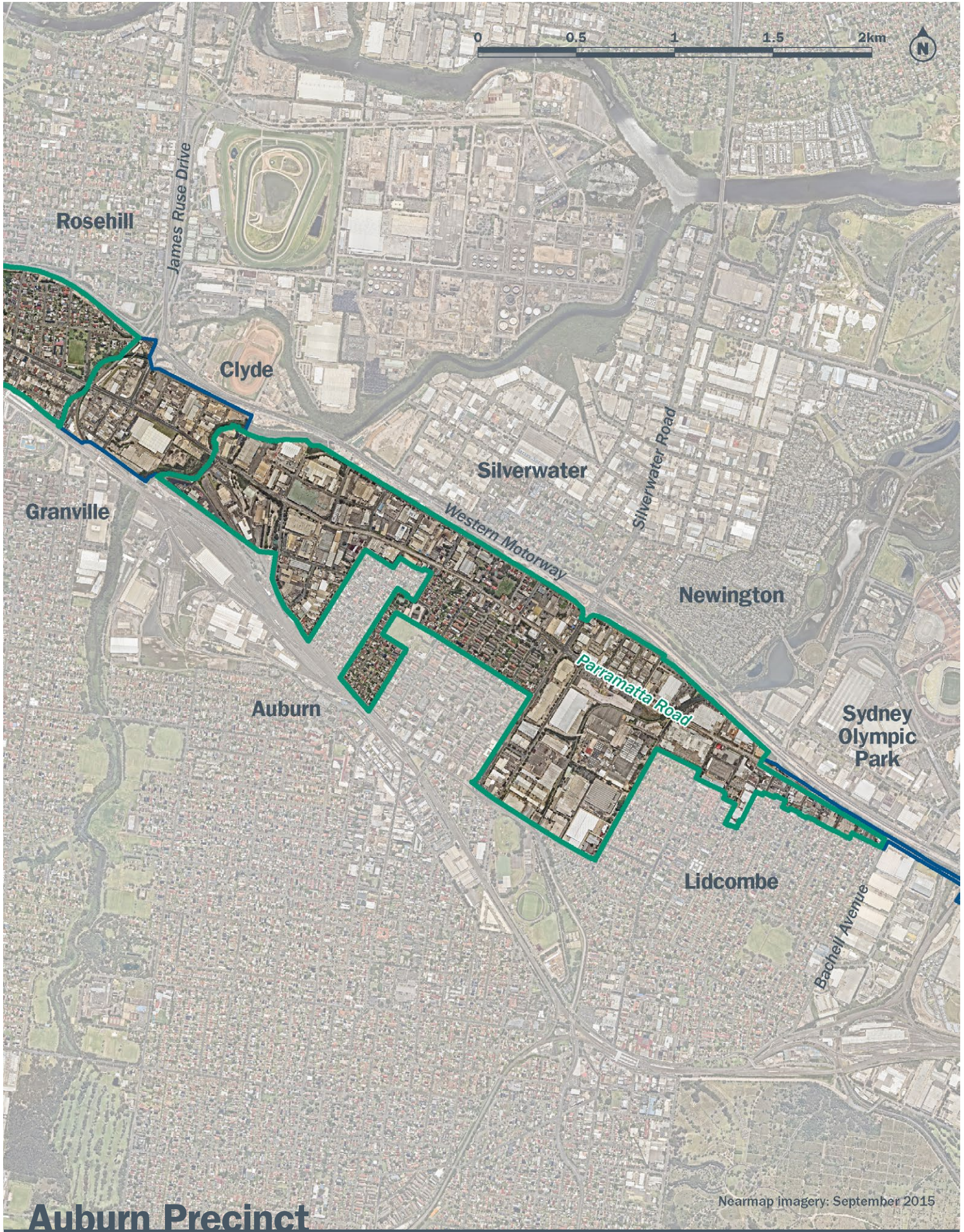
### Social infrastructure and open space

- We are working with the NSW Government Architect's Office and other specialist consultants to determine the optimal location, amount and type of future open space and social infrastructure required to support the renewal of the Precinct.
- We will also continue to work closely with the NSW Department of Education (DOE), NSW Health and Local Health District (LHD) to ensure schools and health facilities can support the future population.
- The Urban Amenity Improvement Program and the Open Space and Social Infrastructure Schedule will be updated to include governance, funding and timing arrangements.



### Impacts to property

- Further community consultation will occur prior to any proposed changes to local zoning and development controls. These changes will be facilitated and approved by DPE, GSC or the local councils. Rezoning will only occur after the adoption of the final Strategy and is intended to be implemented in stages in response to the delivery of key infrastructure.
- In the event that a compulsory land acquisition is needed to deliver infrastructure or open space, this will occur in full consultation with the landowner. UrbanGrowth NSW does not have the power to make compulsory acquisitions. Any land acquisitions would be undertaken by the state or local government agency responsible for delivering the infrastructure or open space.
- We strongly recommend that landowners obtain independent professional advice prior to making any significant decisions about their property.










# Feedback on the Auburn Precinct Plan

**Vision:** Taking advantage of its location close to major employment areas such as Parramatta and Sydney Olympic Park, Auburn can be a location for significant employment growth, supported by moderate scale residential development and an improved streetscape.

Source [draft Parramatta Road Urban Transformation Strategy](#), September 2015. All vision statements were developed with councils.


## Engagement snapshot for the Auburn Precinct

The following table outlines the diverse range of feedback channels and consultation activities<sup>^</sup> that were organised for the Auburn Precinct.

ACTIVITIES	 <b>1</b> shopping centre displays	 <b>1</b> community market pop ups	 <b>1</b> train station pop ups	 <b>2</b> community information and feedback sessions
	RESPONSE CHANNELS	 <b>30*</b> responses to the paper and online surveys	 <b>8</b> focus group participants	 <b>10*</b> community submissions

\* The total number of people who submitted a survey is greater than the number above, as not all participants linked their survey to a Precinct.

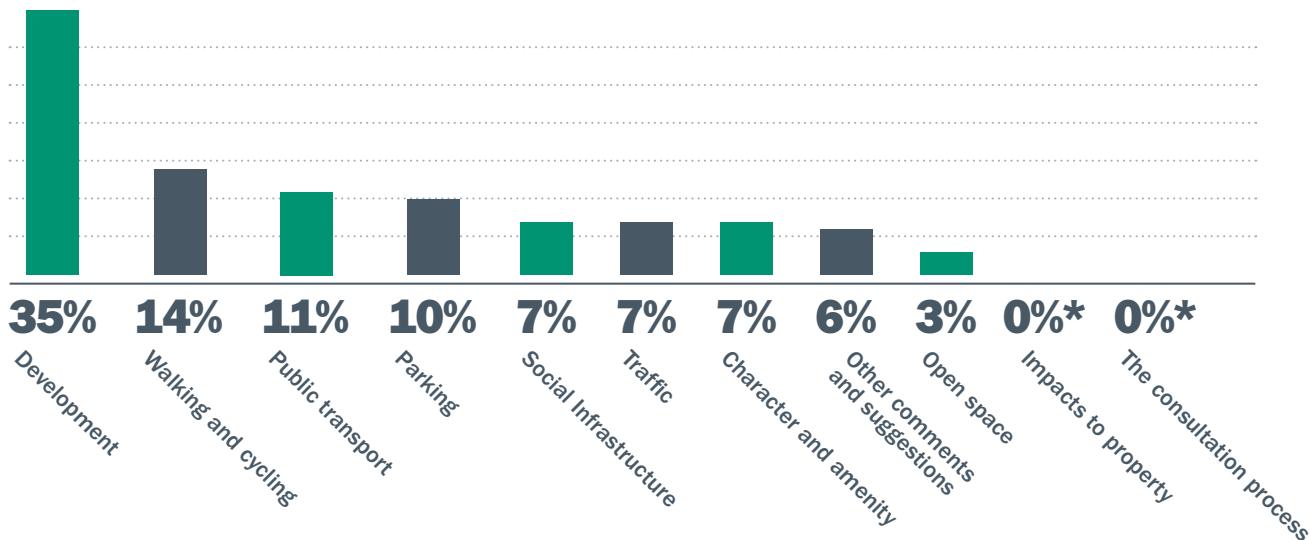
<sup>^</sup> All activities were organised on an LGA basis.



To see more details on the Auburn Precinct Plan please see the draft Parramatta Road Urban Transformation Strategy at [www.urbangrowth.nsw.gov.au](http://www.urbangrowth.nsw.gov.au)

## Feedback on key themes

The graph below presents council and community feedback on the Auburn Precinct Plan against 11 themes. This data is drawn from online/paper surveys and submissions. A breakdown of feedback from councils and the communities is provided in the following sections.



The percentages in the above graph indicate the frequency of comments made by theme.

\* represents less than 0.5% of responses

## What we heard from council

Auburn City Council was the only local council to provide feedback on the Auburn Precinct Plan. A summary of the council's comments and recommendations on the Precinct Plan is provided below.

### Auburn City Council

- Request for the Auburn Precinct to receive a greater share of the \$200 million Urban Amenity Improvement Program funding
- Recommend that economic investment and employment creation be considered as part of the funding allocation model, together with planned increases in residential accommodation and population
- Recommend that connectivity be improved between the Auburn Precinct and adjoining current and planned employment nodes in Silverwater, Carter Street and the Granville Precinct
- Recommend improvements (or new routes, if possible) to walking and cycling routes in Silverwater
- Recommend improvements to walking and cycling facilities at the M4 interchange and the Stubbs Street Bridge as part of the final Urban Amenity Improvement Program
- Suggest road improvements for multiple sections of Parramatta Road, Silverwater Road and St Hilliers Road
- Recommend the Auburn Precinct be considered in the context of Silverwater when finalising plans for the area
- Recommend greater incentives for both housing and economic growth for the Auburn Precinct through adjusting planning controls along Parramatta Road to enable an appropriate mix of uses, including retail/office-commercial/residential floor space

## What we heard from the community

The summary of community feedback on the Auburn Precinct Plan below is presented in order of the themes that received the most comments in the online/paper survey and submissions (as shown in the graph on page 28). This summary reflects the most frequently raised topics under each theme – it is not exhaustive of all comments made on the Auburn Precinct Plan.



### Development

Development was the most frequently discussed theme in the feedback, accounting for 35% of comments made in surveys and submissions.

**Community feedback to the proposed development for the Auburn Precinct was divided evenly between those who supported and opposed the changes.** Other frequent comments included:

- support for increased height, floor space ratio and density along Parramatta Road and near Auburn Station
- request for development to link with local retail key services, the Auburn Town Centre, Auburn Station, employment hubs and social infrastructure
- opposition to increased heights development next to single storey dwellings, such as those on Delhi Street, Bombay Street, Platform Street and Jellicoe Street, and
- concerns from commercial property owners that employment and housing targets will not be met if the current business and enterprise zonings remain.

👉 You've got to provide shops and cafes... you need to attract businesses who want to have offices here, and to have more infrastructure (such as better social and transport infrastructure) to keep them there'.  
– *community feedback*

👉 Enabling for greater residential land use opportunities will take advantage of the projected growth in jobs within the area, by allowing more residents to live closer to employment areas'.  
– *community feedback*



### Walking and cycling

Walking and cycling accounted for 14% of comments made in surveys and submissions.

**The most frequent matter raised was the need to ensure the cycling and walking pathways provided direct connections to key local destinations such as Bunnings, Costco, Reading Cinema, Lidcombe Community Centre, Wentworth Point, and Auburn Station.**

Other comments included:

- support for improvements to pedestrian and cyclist amenity
- requests that cyclist and pedestrian safety is a high priority, and
- requests to connect active transport links to increased heights areas.



### Public transport

Public transport accounted for 11% of comments made in surveys and submissions. **The majority of feedback focused on the need for public transport improvements to support the projected population increase.**

👉 ... increased building heights provide an opportunity to ensure high quality public domain outcomes across the site. Pedestrian amenity and improved bus stop facilities will be key to achieving an integrated transport solution...'  
– *community feedback*

 **Parking**

Parking accounted for 10% of comments made in surveys and submissions. Most of the comments focused on the **need for more parking in the area**, particularly off-street parking and commuter parking at Auburn Station.

 **Social infrastructure**

Social infrastructure accounted for 7% of comments made in surveys and submissions. Participants said there was a need for **more social infrastructure such as schools, childcare and health services to support population growth**.

 **Traffic**

Traffic accounted for 7% of comments in surveys and submissions, with most people raising **concerns about increased population compounding existing congestion**.

📌 When on-site car parking is not provided the streets get cluttered with overflow parking from units. Wentworth Point demonstrates the failure of not providing adequate parking’.  
– *community feedback*

 **Character and amenity**

Character and amenity accounted for 7% of comments made in surveys and submissions. **The feedback welcomed public amenity improvements for the Precinct, with a preference for landscaping, streetscaping, gathering spaces and more public toilets**. Feedback also showed that the cultural diversity of Auburn was highly valued by the community.

 **Other**

Feedback that could not be accommodated into other listed themes accounted for 6% of comments made in surveys and submissions. Most of these comments were from people stating that the Precinct Plan was unclear and difficult to understand.

 **Open space**

Open space accounted for 3% of comments made in surveys and submissions, with most feedback requesting more open space for the Auburn Precinct.

 **Impacts to property**

Impacts to property accounted for less than 0.5% of comments made in surveys and submissions.

 **Consultation process**

Feedback on the consultation program accounted for less than 0.5% of comments made in surveys and submissions. Suggestions were made for future communications about the Strategy, including:

- letterbox drops
- notices in local libraries
- communications through Wentworth Point Community Central
- information in community languages, and
- consultation with the surrounding communities of Wentworth Point and Sydney Olympic Park.



## Our initial response

Further community consultation will occur prior to any proposed changes to local zoning and development controls. This consultation will occur in line with statutory processes. Where appropriate, key components of the Strategy will be included within the District Plans presently being developed by Greater Sydney Commission (GSC), Department of Planning and Environment (DPE) and councils. The District Plans will then be implemented through councils' Local Environmental Plans (LEPs) in coordination with the delivery of key infrastructure.

Below we have provided our initial responses to the matters most frequently raised in the feedback and outline how we will finalise the Strategy in collaboration with local councils and government agencies.



### Development

- The overall vision and function of the Auburn Precinct will be re-examined. Council, UrbanGrowth NSW, DPE and GSC, through the relevant District Plan, will analyse and determine if Auburn Precinct should:
  - be identified primarily as an economic and employment centre with limited residential uses located close to Auburn Town Centre and the train station as outlined in the draft Strategy.
  - OR
  - whether there is potential to introduce new residential and mixed uses along Parramatta Road and throughout the Precinct, appropriately supported by the further provision of public transport, open space and social infrastructure.

If this is not resolved in the Strategy it will be addressed in the district planning process.

- A consultant team will assist to review and refine the land uses, scale and access and movement principles. This will include potential mechanisms to incentivise housing and economic growth through adjusted planning controls.
- The consultant team will also revise the draft Urban Design Guidelines in conjunction with revisions to the Precinct Plans, and specifically respond to community feedback requesting more detailed controls that guide transitions between taller buildings and low density development at the Precinct edges. Particular focus will be on Delhi, Jellicoe, Bombay and Platform Streets as identified in submissions.



### Public and active transport and traffic

- All council and community feedback about transport services will be provided to Transport for NSW (TfNSW) and Roads and Maritime Services (RMS) to inform their public transport and roads planning along the Corridor.
- Rezoning of land will be triggered by mechanisms that align the coordination and delivery of services and infrastructure as identified by DPE and GSC.
- We will examine potential intersection upgrades, road improvements, walking and cycling connections.



### Character and amenity

- The local amenity improvement works proposed as part of the Urban Amenity Improvement Program (UAIP) will need to be reconsidered. Whilst the criteria for determining UAIP works will remain unchanged, the future desired character of the Auburn Precinct may support additional works that could be funded under the draft UAIP.



### Impacts to property

- Further community consultation will occur prior to any proposed changes to local zoning and development controls. These changes will be facilitated and approved by DPE, GSC or the local councils. Rezoning will only occur after the adoption of the final Strategy and is intended to be implemented in stages in response to the delivery of key infrastructure.
  - In the event that a compulsory land acquisition is needed to deliver infrastructure or open space, this will occur in full consultation with the landowner. UrbanGrowth NSW does not have the power to make compulsory acquisitions. Any land acquisitions would be undertaken by the state or local government agency responsible for delivering the infrastructure or open space.
  - We strongly recommend that landowners obtain independent professional advice prior to making any significant decisions about their property.
- 



### Social infrastructure and open space

- We are working with the NSW Government Architect's Office and other specialist consultants to determine the optimal location, amount and type of future open space and social infrastructure required to support the renewal of the Precinct.
  - We will also continue to work closely with the NSW Department of Education (DOE), NSW Health and Local Health District (LHD) to ensure schools and health facilities can support the future population.
  - The Urban Amenity Improvement Program and the Open Space and Social Infrastructure Schedule will be updated to include governance, funding and timing arrangements.
-



# Feedback on the Homebush Precinct Plan

**Vision:** Sitting between Sydney’s two main CBDs, Homebush can be transformed into an active and varied hub, blending higher density housing and a mix of different uses, supported by a network of green links and open spaces with walking access to four train stations.

Source [draft Parramatta Road Urban Transformation Strategy](#), September 2015. All vision statements were developed with councils.

## Engagement snapshot for the Homebush Precinct

The following table outlines the diverse range of feedback channels and consultation activities^ that were organised for the Homebush Precinct.

 <p><b>1</b> shopping centre displays</p>	 <p><b>1</b> community market pop up</p>	 <p><b>1</b> train station pop up</p>	 <p><b>1</b> community information and feedback sessions</p>	<b>ACTIVITIES</b>  <b>RESPONSE CHANNELS</b>
 <p><b>125*</b> responses to the paper and online survey</p>	 <p><b>12</b> focus group participants</p>	 <p><b>120</b> community submissions</p>	 <p><b>2</b> council submissions</p>	

\* The total number of people who submitted a survey is greater than the number above, as not all participants linked their survey to a Precinct.

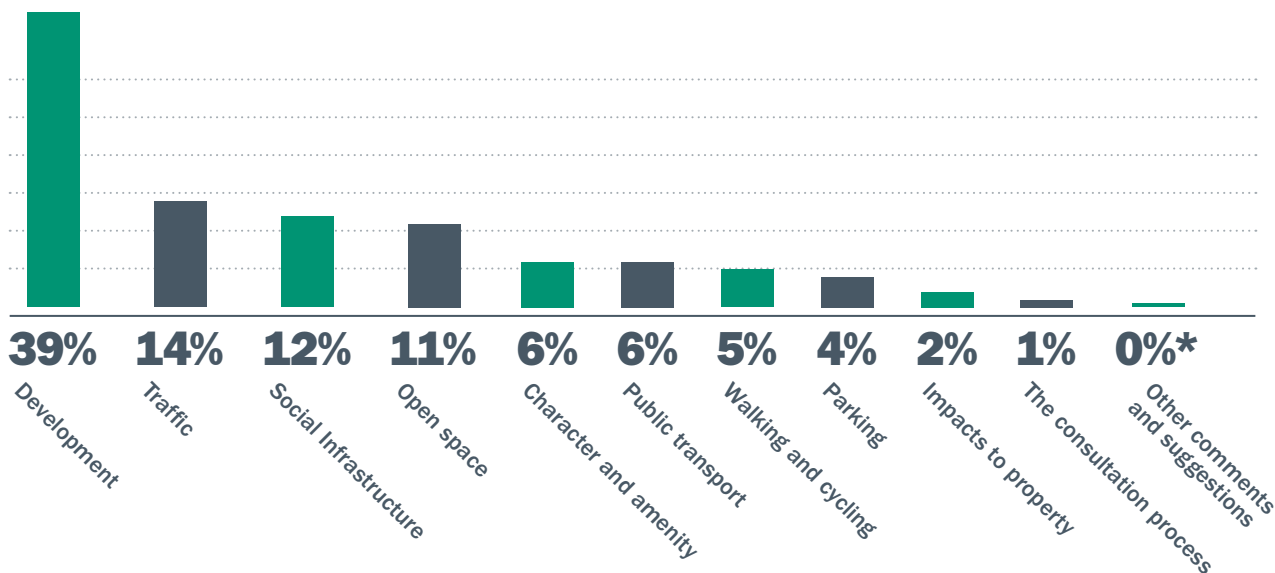
^ All activities were organised on an LGA basis.



To see more details on the Homebush Precinct Plan please see the draft Parramatta Road Urban Transformation Strategy at [www.urbangrowth.nsw.gov.au](http://www.urbangrowth.nsw.gov.au)

## Feedback on key themes

The graph below presents council and community feedback on the Homebush Precinct Plan against 11 themes. This data is drawn from online/paper surveys and submissions. A breakdown of feedback from councils and the communities is provided in the following sections.



The percentages in the above graph indicate the frequency of comments made by key theme.

\* represents less than 0.5% of responses

## What we heard from councils

Council submissions on the Homebush Precinct Plan were made by the City of Canada Bay Council and Strathfield Municipal Council. A summary of their comments and recommendations is provided below.

### City of Canada Bay

- State strongly that there are three fundamental issues that need to be resolved and that are critical to the implementation of the Strategy. They are:
  - transport
  - affordable housing, and
  - value capture
- Request no rezoning north of Pomeroy Street until the City of Canada Bay draft Master Plan for Concord West Precinct is finalised
- Recommend the development standards for the Bakehouse Quarter be informed by a Heritage Assessment
- Recommend that building heights on the eastern side of Swan Avenue be reduced to a maximum of two to three storeys and building heights on the western side of Swan Avenue and on Manson Road be reduced to a maximum height of six storeys
- Request the NSW Government provides a site specific Development Control Plan that considers the building form and heritage matters in the Precinct, particularly in the Bakehouse Quarter
- Request that guidelines be developed to specify minimum acoustic standards for future developments in greater detail
- Support the proposed maximums for off street car parking and other initiatives such as car sharing to reduce car ownership and traffic generation
- Request the prioritising of an integrated transport system with effective linkages between bus, rail and ferry services
- Request that the Strategy indicate the location of the new multipurpose community facility
- Recommend an investigation into ways of providing greater community access to the Yaralla Estate and the Dame Edith Walker Estate for community recreation and sporting activities

## Strathfield Municipal Council

- Recommend the proposed development adapt to the existing local characteristics and heritage elements
- Believe that the Homebush Precinct is receiving an unfair proportion of the development proposed in the draft Strategy
- Recommend detailed capacity modelling to ascertain the cumulative impact of the proposed 17,354 new dwelling targets in the Homebush Precinct
- Believe the proposed built form guidelines are not comprehensively based on sound spatial strategic planning
- Identify the proposed differential in building height and floor space ratio from larger sites to smaller sites will result in an ad hoc overall built form across the Corridor
- Recommend that density and height be based on proximity to rail corridors and/or other key transport nodes
- Recommend lot size incentives be limited to allow for a well-considered and logical spatial hierarchy of built form
- Concern that the proposed heights along the Ismay Avenue and southern section of Underwood Road do not have a supporting flood study
- Request more emphasis on catering for the additional traffic capacity, including major road upgrades
- Request more detailed information on traffic and parking
- Concern that proposed restrictive parking rates for new development will increase on-street parking demand
- Concern that many areas farthest from train stations have been assigned the highest development densities
- Believe the additional open space provision is inadequate for the population and employment growth targets
- Note the indicative light rail route might have impacts on existing regional open space and sports fields
- Concern the proposed vehicular connection through the southern boundary of Mason Park Wetlands will impact the wetlands and vulnerable species
- Concern that social infrastructure is not in place prior to population increases
- Concern that key infrastructure facilities including senior/youth centres, community centres and indoor sport and recreation centres are not proposed in the draft Strategy
- Concern the proposed funding mechanism relies too heavily on Council Section 94 Plans
- Recommend the introduction of a value sharing funding mechanism
- Request the proposed urban amenity improvements include a pedestrian overpass over Parramatta Road
- Request that employment growth projections exclude North Strathfield
- Concern the draft Strategy overestimates job increases in the Precinct
- Request more details about the Homebush Main Trade Area Recommended Retail Floorspace Demand 2015-54

## What we heard from the community

The summary of community feedback on the Homebush Precinct Plan below is presented in order of the themes that received the most comments in the online/paper survey and submissions (as shown in the graph on page 35). This summary describes the most frequently raised topics under each theme – it is not exhaustive of all comments made on the Homebush Precinct Plan.



### Development

Development was the most frequently discussed theme in the feedback, accounting for 39% of comments made in surveys and submissions.

**The majority of feedback opposed the proposed heights of development in the Precinct, particularly in areas adjacent to single storey residential dwellings such as Lorraine Street, George Street, Swan Avenue, Manson Road and north of Pomeroy Street.**

Other comments included:

- requests that new developments are built using high quality architectural standards
- requests building heights do not exceed eight storeys
- support for higher density development around train stations, Parramatta Road, the southern side of the M4 and around the industrial/commercial area near Mason Park
- suggestions that the draft Strategy include more commercial, retail and business areas
- interest in new developments providing additional affordable housing, and
- support from some landowners for increased heights, densities, floor space ratios and mixed use zonings on their land.

📌 The Draft Parramatta Road Plan should only relate to Parramatta Road instead this plan only generates unsustainable pockets of density in the suburbs-unacceptable! The inner west is not a dumping ground for all future development and population growth’.

– *community feedback*

📌 How are those now living in a 2a residential area to have their lives improved through a diminishment in sunlight and privacy? And what about the noise accruing from construction vehicles and sites? What about those who actually enjoy living in a single storey developed area? Are we seen as pawns simply to be pushed aside? Though it might come as a surprise to you our home, community and history is here’.

– *community feedback*

📌 Density done well can contribute to new life along this Corridor and at the same time can remain sensitive to the character and heritage of the area’.

– *community feedback*

## Traffic

Traffic accounted for 14% of comments made in surveys and submissions, with **most people raising concerns about increased population compounding existing congestion, particularly on George Street, Pomeroy Street, Underwood Road, Homebush Bay Drive and Australia Avenue.** Other comments included:

- concerns about the proposed road access to Lorraine Street across Mason Park and the potential environmental impact of increased traffic to the Mason Park Wetlands
- suggestions for additional entry/exit points on George St, and
- requests for better north south connections and improved access to Homebush Bay Drive.

📌 Right now George Street, Pomeroy Street, Underwood Road, Homebush Bay Drive and Australia Avenue are unable to cope with the volume of motor traffic during peak times and the situation is certain to become dramatically worse if the proposed huge increase of the population of the area goes ahead'.  
 – *community feedback*

## Social infrastructure

Social infrastructure accounted for 12% of comments made in surveys and submissions. Participants said there was a need for **more social infrastructure to support population growth, such as schools and health services.**

📌 Without the infrastructure in place the project will be set to fail. Although there is demand for property in the area the residents will suffer unless the planning around facilities is undertaken properly without only taking developers (sic) needs into consideration'.  
 – *community feedback*

## Open space

Open space accounted for 11% of comments made in surveys and submissions, with most feedback stating **more open space is needed** for the Homebush Precinct. Other comments included:

- concerns about loss of existing open space including the potential loss of the grass area and bird sanctuary beside the Powells Creek canal, and
- suggestions to widen the land surrounding Powells Creek to create a green corridor.

## Character and amenity

Character and amenity accounted for 6% of comments made in surveys and submissions. **The majority of feedback highlighted concerns about the proposed developments' negative impact on the local character and heritage, particularly Mosely Street, Roberts Street, Swan Avenue and Manson Road.** The proposed urban amenity improvements were well received by the community as was the general vision to revitalise the area. Other comments included:

- requests that plans involving Bakehouse Quarter be informed by a heritage assessment
- suggestions for amenity improvements to footpaths, street planting, undercover BBQ and seating areas, and seating along walking/cycling pathways, and
- support for more restaurants, shops and businesses in the area.

📌 The draft plan will decimate the Concord community, changing the fundamental nature of Concord. To convert this parkland suburb into soulless canyons of concrete is criminal. This vibrant and caring community is characterised by war service homes and families, backyard cricket and BBQs. Every home has the space for a netball ring, a vegetable garden and a chook run, a peach or mango tree, a place for pets to grow and thrive. Concord is the home of grey nomads and their caravans. Concord is a fisherman's paradise. Under the draft Plan there will be nowhere to park a car, let alone a boat or a van'.  
 – *community feedback*



 **Public transport**

Public transport accounted for 6% of comments made in surveys and submissions. The majority of feedback focused on **the need for public transport improvements to support the projected population increases**. Other comments included:

- requests for a more integrated transport system, with better links between bus, rail and ferry services
- requests for increasing the frequency of train services, and
- requests for a firm commitment to public transport funding prior to the construction of new development.

 **Walking and cycling**

Walking and cycling accounted for 5% of comments made in surveys and submissions, with **most people commenting on the need to improve pedestrian and cyclist pathways with a focus on safety**. A number of suggestions were made to improve walking and cycling in the area including:

- a pedestrian/cycle bridge over Parramatta Road
- a separated cycleway along Parramatta Road
- pathway along Powells Creek
- a dedicated cycleway into the Bicentennial Park region from Parramatta Road
- more pedestrian crossings along George Street
- a pedestrian bridge across the railway that connects Queen Street with Railway Street, and
- greater access across the train line to Concord Road to access bus transport along Concord Road.

 **Parking**

Parking accounted for 4% of comments made in surveys and submissions. Most of the participants said there were existing parking shortages in the area and a need for more parking to accommodate population growth.

 **Impacts to property**

Impacts to property accounted for 2% of comments made in surveys and submissions. Most comments were from landowners concerned about compulsory acquisitions and reductions in local property values.

 **Consultation process**

Feedback on the consultation program accounted for 1% of comments made in surveys and submissions. The majority of feedback was from people requesting more detail on the Precinct Plan.

 **Other**

Feedback that could not be accommodated into the other listed themes accounted for less than 0.5% of comments made in surveys and submissions. Comments included concerns about flood management along Powells Creek.

## Our initial response

Further community consultation will occur prior to any proposed changes to local zoning and development controls. This consultation will occur in line with statutory processes. Where appropriate, key components of the Strategy will be included within the District Plans presently being developed by Greater Sydney Commission (GSC), Department of Planning and Environment (DPE) and councils. The District Plans will then be implemented through councils' Local Environmental Plans (LEPs) in coordination with the delivery of key infrastructure.

Below we have provided our initial responses to the matters most frequently raised in the feedback and outline how we will finalise the Strategy in collaboration with local councils and government agencies.

### Development

- We will re-examine elements of the Precinct Plan, taking account of the feedback received and the recently announced preferred route for the Parramatta Light Rail.
- We will reinvestigate the overall vision and character between the Bakehouse Quarter and the Precinct's northern boundary.
- We have engaged a heritage consultant and architect to work with our urban designer. Together, this team will:
  - map all existing heritage items and heritage conservation areas across the entire Corridor for inclusion in the final Strategy
  - recommend additional properties and landscapes that could be considered for heritage listing or protection, and
  - review and refine heritage controls for inclusion in the Urban Design Guidelines, with particular regard to heritage floor space transfers, minimum setbacks, integration and transition principles.

### Character and amenity

- We are working with the NSW Government Architect's Office and other specialist consultants to determine the optimal location, amount and type of future open space and social infrastructure required to support the renewal of the Precinct.
- We will also continue to work closely with the Department of Education (DOE), NSW Health and Local Health Districts (LHD) to ensure schools and health facilities can support the future population.

### Public and active transport and traffic

- All council and community feedback about to transport services will be provided to Transport for NSW (TfNSW) and Roads and Maritime Services (RMS) to inform their public transport and roads planning along the Corridor.
- Rezoning of land will be triggered by mechanisms that align the coordination and delivery of services and infrastructure as identified by DPE and GSC.
- Working with TfNSW, RMS, Strathfield Municipal Council and City of Canada Bay Council, we will continue to investigate the public transport options currently being explored. The final Strategy will consider the implications of the recently announced Parramatta Light Rail.
- We will investigate intersection upgrades, road improvements and walking and cycling connections. In particular, analysing George Street, Pomeroy Street, Underwood Road, Homebush Bay Drive and Australia Avenue.
- The implications of a new north-south road parallel to Powells Creek Reserve and the road access to Lorraine Street across Mason Park will be investigated.
- The local amenity improvement works proposed in the Urban Amenity Improvement Program (UAIP) will be reconsidered. As part of this, the active transport links suggested by local councils and communities will be assessed against the established UAIP criteria.

- A survey will be commissioned to better understand where and how parking is used, particularly around existing or planned transport nodes. The results from this survey will be used to inform planning controls and other measures to reduce car ownership and promote the use of public transport.

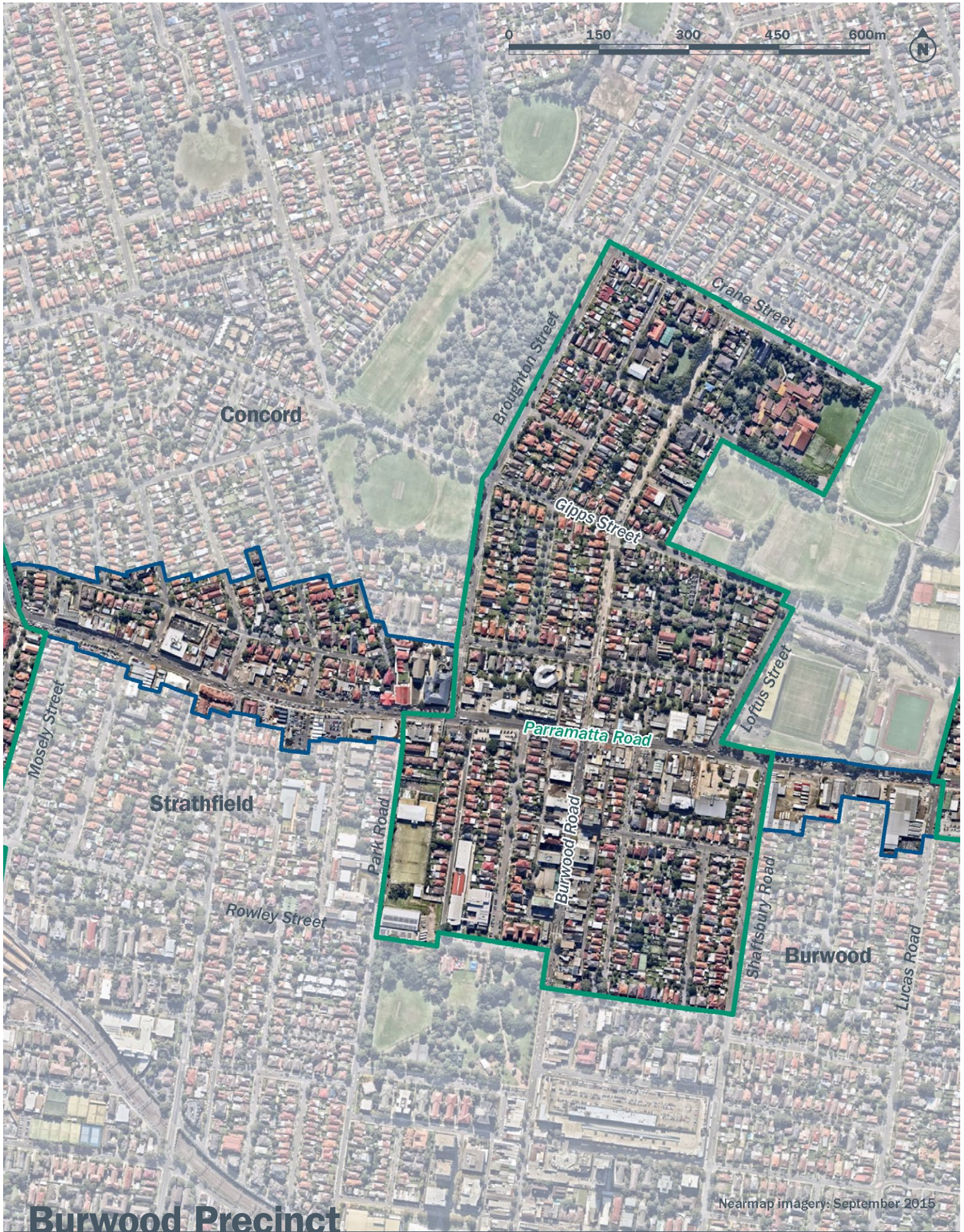
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 **Impacts to property**

- Further community consultation will occur prior to any proposed changes to local zoning and development controls. These changes will be facilitated and approved by DPE, GSC or the local councils. Rezoning will only occur after the adoption of the final Strategy and is intended to be implemented in stages in response to the delivery of key infrastructure.
- In the event that a compulsory land acquisition is needed to deliver infrastructure or open space, this will occur in full consultation with the landowner. UrbanGrowth NSW does not have the power to make compulsory acquisitions. Any land acquisitions would be undertaken by the state or local government agency responsible for delivering the infrastructure or open space.
- We strongly recommend that landowners obtain independent professional advice prior to making any significant decisions about their property.

 **Social infrastructure and open space**

- We are working with the NSW Government Architect's Office and other specialist consultants to determine the optimal location, amount and type of future open space and social infrastructure required to support the renewal of the Precinct.
  - We will also continue to work closely with the NSW Department of Education (DOE), NSW Health and Local Health District (LHD) to ensure schools and health facilities can support the future population.
  - The Urban Amenity Improvement Program and the Open Space and Social Infrastructure Schedule will be updated to include governance, funding and timing arrangements.
-



 Precinct Area     Frame Area

# Feedback on the Burwood Precinct Plan

**Vision:** Burwood Precinct will be a commercial gateway to Burwood Town Centre based around the enlivened spine of Burwood Road building upon existing amenity for new residents.

Source [draft Parramatta Road Urban Transformation Strategy](#), September 2015. All vision statements were developed with councils.


## Engagement snapshot for the Burwood Precinct

The following table outlines the diverse range of feedback channels and consultation activities<sup>^</sup> that were organised for the Burwood Precinct.

ACTIVITIES				
	<b>1</b> shopping centre display	<b>1</b> community market pop up	<b>1</b> community information and feedback sessions	
RESPONSE CHANNELS				
	<b>242*</b> responses to the paper and online survey	<b>9</b> focus group participants	<b>236</b> community submissions	<b>2</b> council submissions

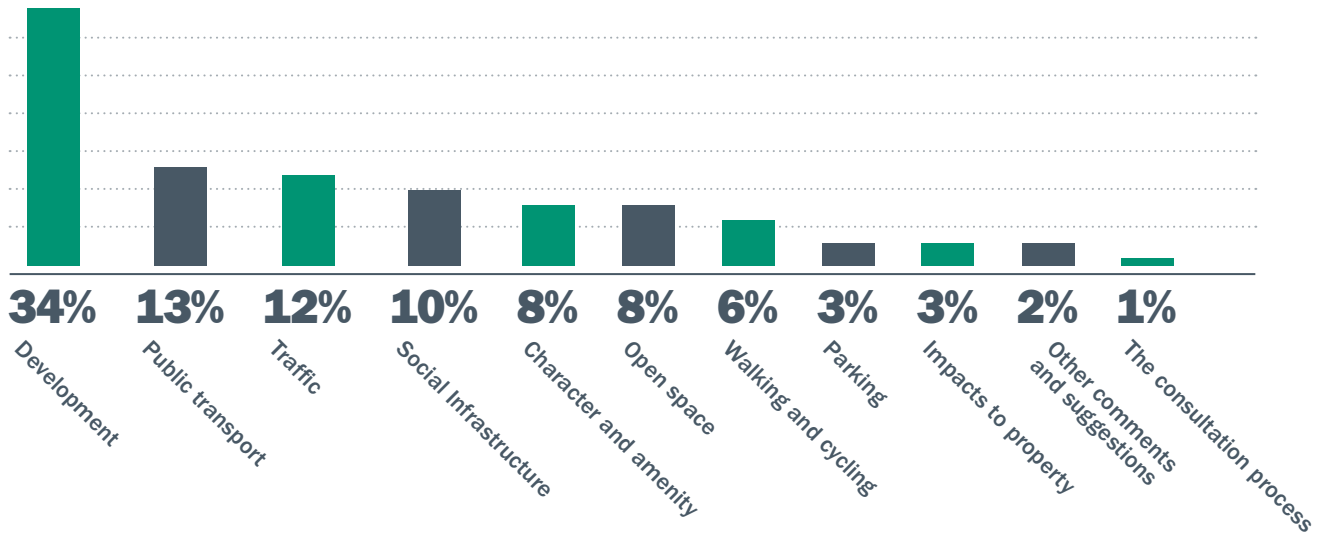
\* The total number of people who submitted a survey is greater than the number above, as not all participants linked their survey to a Precinct.

<sup>^</sup> All activities were organised on an LGA basis.

 To see more details on the Burwood Precinct Plan please see the draft Parramatta Road Urban Transformation Strategy at [www.urbangrowth.nsw.gov.au](http://www.urbangrowth.nsw.gov.au)

## Feedback on key themes

The graph below presents council and community feedback on the Burwood Precinct Plan against 11 themes. This data is drawn from online/paper surveys and submissions. A breakdown of feedback from councils and the communities is provided in the following sections.



The percentages in the above graph indicate the frequency of comments made by key theme.

## What we heard from councils

Council submissions on the Burwood Precinct Plan were made by the City of Canada Bay Council and Burwood Council. A summary of their comments and recommendations is provided below.

### City of Canada Bay

- State strongly that there are three fundamental issues that need to be resolved and that are critical to the implementation of the Strategy. They are:
  - transport
  - affordable housing, and
  - value capture
- Support for the revitalisation of the Corridor, in particular for providing public and active transport links in the Precincts
- Support for the proposed land use mix, including residential uses on the northern side of Parramatta Road
- Recommend altering maximum height limits at a number of locations including Broughton Street, Gipps Street, Burwood Road, Burton Street, Stanley Street, Crane Street, Concord High School, Lukes Oval, Gipps Street, Burwood Road, Gipps Street, and Loftus Street
- Support the proposed maximums for off-street car parking and other initiatives such as the promotion of car share programs
- Request an investigation into the upgrade of Parramatta Road and Burwood Road
- Request an integrated transport network be a priority
- Request the reinstatement of the Bayview ferry service
- Request further analysis and review of heritage in the Burwood Precinct
- Request that floor space ratio transfer be considered where heritage items are to be retained
- Recommend future development controls be informed by advice from a qualified heritage professional
- Recommend that the Precinct balances development with heritage conservation
- Request that detailed guidelines be established that specify minimum acoustic standards for future developments

### Burwood Council

- Require further detail about frame areas and the planning framework proposed for these areas
- Request more certainty around public transport, transport Super Stops, landmark buildings, and traffic and parking management
- Request further detail about the draft Strategy's implementation, particularly:
  - how new open spaces will be acquired, funded and staged
  - how new mechanisms for funding public infrastructure will be developed
  - how housing diversity outcomes will be achieved along the Corridor
  - how new community infrastructure and upgrades will be funded
  - how urban amenity improvements will be funded and staged
  - how Council is expected to treat planning proposals in the interim
- Note that council parking rates and several heritage items have been incorrectly identified in the Precinct Plan
- Concern that restrictions in the Burwood Precinct Structure Plan and the Burwood Local Environmental Plan may stifle development
- Request the treatment of the sensitive Precinct edge be given legislative weight to ensure its implementation
- Oppose the 25 storey building heights proposed at the intersection of Burwood Road and Parramatta Road\*
- Oppose the Transport Report's recommendation for maximum parking rates<sup>^</sup>
- Request that provisions be made for the increased demand for parking to support proposed increase in retail and commercial development
- Request funding is provided to acquire land for the provision of off-street parking
- Recommend that the impacts created by an increase in population on existing community facilities be given further consideration
- Recommend increases in frequency and capacity of train services on the Western Line at Burwood Railway Station

\* since this is greater than the heights permitted at the core of Burwood Town Centre

<sup>^</sup> recommend the Transport Report stipulate minimum parking rates rather than maximum, since they would allow Council to better manage and monitor parking over time

## What we heard from the community

The summary of community feedback on the Burwood Precinct Plan below is presented in order of the themes that received the most comments in the online/paper survey and submissions (as shown in the graph on page 44). This summary reflects the most frequently raised topics under each theme – it is not exhaustive of all comments made on the Burwood Precinct Plan.

### Development

Development was the most frequently discussed theme in the feedback, accounting for 34% of comments made in surveys and submissions. **The majority of participants considered the proposed heights for development as inappropriate for the Precinct, particularly on Broughton Street, Burton Street and north of Gipps Street.** Other comments included:

- requests that development be guided by good design
- concern about loss of privacy and overshadowing from tall buildings, particularly close to schools
- support for the provision of more affordable housing by increasing housing density, and
- support for the proposed land use, heights and development density along Parramatta Road and in the Burwood Town Centre.

‘I encourage growth but keep the high rise on Parramatta Road. Integrate with shopping, improved transport etc, improved pedestrian access; make Parramatta Road a living and shopping hub!!!’

– *community feedback*

‘We can understand and support the logic for high density development along the Parramatta Road Corridor itself, but to propose high density development to penetrate so deeply into the suburb of Concord, i.e. the Crane Road boundary, will be a disaster for the community and the suburb of Concord’.

– *community feedback*

‘We understand that there is a need for development to continue happening in Sydney to support the population growth that is expected, and to continue with the new infrastructure plans that are being rolled out across the state’.

– *community feedback*

### Public transport

Public transport accounted for 13% of comments made in surveys and submissions. The majority of feedback focused on the **need for increasing the frequency and capacity of public transport** to support the projected population increases.

‘Sydney’s unaffordable housing market seems to be getting worse. It would be good to increase supply of high density housing to reduce cost. But also increase supply of public transport and services to meet the increase in population’.

– *community feedback*



## Traffic

Traffic accounted for 12% of comments made in surveys and submissions, with **most people raising concerns about increased population compounding existing congestion**. Other comments included:

- concerns that car use in the area will remain high even if public transport is improved, and
- requests for improvements to the intersection of Parramatta Road and Burwood Road.

👉 To say people will use public transport and not use cars is unrealistic'.  
– *community feedback*

## Social infrastructure

Social infrastructure accounted for 10% of comments made in surveys and submissions. Participants said there was a need for **more social infrastructure to support population growth**, such as schools and health services.

## Character and amenity

Character and amenity accounted for 8% of comments made in surveys and submissions. **The majority of feedback highlighted concerns about the proposed development's negative impact on the local character and heritage**. Other comments included:

- support for more street trees/greenery, and
- requests that further analysis on local heritage be done prior to development.

## Open space

Open space accounted for 8% of comments made in surveys and submissions, with most feedback requesting **more open space for the Precinct**. Some participants objected to the proposed locations.

## Walking and cycling

Walking and cycling accounted for 6% of comments made in surveys and submissions, with most people commenting on the need to improve pedestrian and cyclist pathways with a focus on safety.

## Parking

Parking accounted for 3% of comments made in surveys and submissions. Most of the participants said there were existing parking shortages in the area and a need for more parking to accommodate population growth, including commuter parking and off-street parking for new developments.

## Impacts to property

Impacts to property accounted for 3% of comments made in surveys and submissions. Most comments were from landowners regarding compulsory acquisitions and reductions in local property values.

## Other

Feedback that could not be accommodated into the other themes accounted for 2% of comments made in surveys and submissions. Comments included requests for more detail about the funding and implementation of the Strategy, and requests to rename the Precinct to reflect its location.

## Consultation process

Feedback on the consultation program accounted for 1% of comments made in surveys and submissions. The majority of feedback was from people requesting more detail on the Precinct Plan.

## Our initial response

Further community consultation will occur prior to any proposed changes to local zoning and development controls. This consultation will occur in line with statutory processes. Where appropriate, key components of the Strategy will be included within the District Plans presently being developed by Greater Sydney Commission (GSC), Department of Planning and Environment (DPE) and councils. The District Plans will then be implemented through councils' Local Environmental Plans (LEPs) in coordination with the delivery of key infrastructure.

Below we have provided our initial responses to the matters most frequently raised in the feedback and outline how we will finalise the Strategy in collaboration with local councils and government agencies.



### Development

- We have engaged a consultant team to review the land uses, scale and access and movement principles. The proposed heights and densities north of Gipps Street in Concord will be reinvestigated, with a view to limiting building heights in this area to those allowed under the current planning controls.
- The consultant team will refine the draft Strategy and Urban Design Guidelines to support design excellence in future built form, public domain and private amenity.
- We are working closely with State Government agencies to finalise the Government's position on housing affordability to ensure the final Strategy is consistent with broader NSW Government policy.
- We are proposing to rename the Precinct 'Burwood/Concord'.



### Social infrastructure, character and amenity

- We are working with the NSW Government Architect's Office and other specialist consultants to confirm the optimal location, amount and type of future open space and social infrastructure required to support the renewal of the Burwood Precinct.
- We will also continue to work closely with the Department of Education (DOE), NSW Health and the Local Health District (LHD) to ensure schools and health facilities can support the future population.



### Impacts to property

- Further community consultation will occur prior to any proposed changes to local zoning and development controls. These changes will be facilitated and approved by DPE, GSC or the local councils. Rezoning will only occur after the adoption of the final Strategy and is intended to be implemented in stages in response to the delivery of key infrastructure.
- In the event that a compulsory land acquisition is needed to deliver infrastructure or open space, this will occur in full consultation with the landowner. UrbanGrowth NSW does not have the power to make compulsory acquisitions. Any land acquisitions would be undertaken by the state or local government agency responsible for delivering the infrastructure or open space.
- We strongly recommend that landowners obtain independent professional advice prior to making any significant decisions about their property.



### **Public and active transport and traffic**

- Rezoning of land will be triggered by mechanisms that align the coordination and delivery of services and infrastructure as identified by DPE and GSC.
- All council and community feedback about transport services will be provided to Transport for NSW (TfNSW) and Roads and Maritime Services (RMS) to inform their public transport and roads planning along the Corridor.
- Working with TfNSW, RMS, and both local councils (Burwood and City of Canada Bay), we will continue to investigate the public transport options currently being explored.
- Traffic movements and intersection performance will be analysed in more detail.
- A parking survey will be commissioned to better understand where and how parking is used, particularly around existing or planned transport nodes to inform planning controls and promote the use of public transport.



Nearmap imagery: September 2015

 Precinct Area     Frame Area

# Feedback on the Kings Bay Precinct Plan

**Vision:** Kings Bay will be a new residential and mixed use urban village on Parramatta Road, with an active main street and strong links to the open space network along Sydney Harbour.

**Source** [draft Parramatta Road Urban Transformation Strategy](#), September 2015. All vision statements were developed with councils.


## Engagement snapshot for the Kings Bay Precinct

The following table outlines the diverse range of feedback channels and consultation activities^ that were organised for the Kings Bay Precinct.

ACTIVITIES	 <p><b>1</b> shopping centre display</p>	 <p><b>1</b> community market pop up</p>	 <p><b>1</b> community information and feedback sessions</p>	
RESPONSE CHANNELS	 <p><b>97*</b> responses to the paper and online survey</p>	 <p><b>11</b> focus group participants</p>	 <p><b>96</b> community submissions</p>	 <p><b>2</b> councils submissions</p>

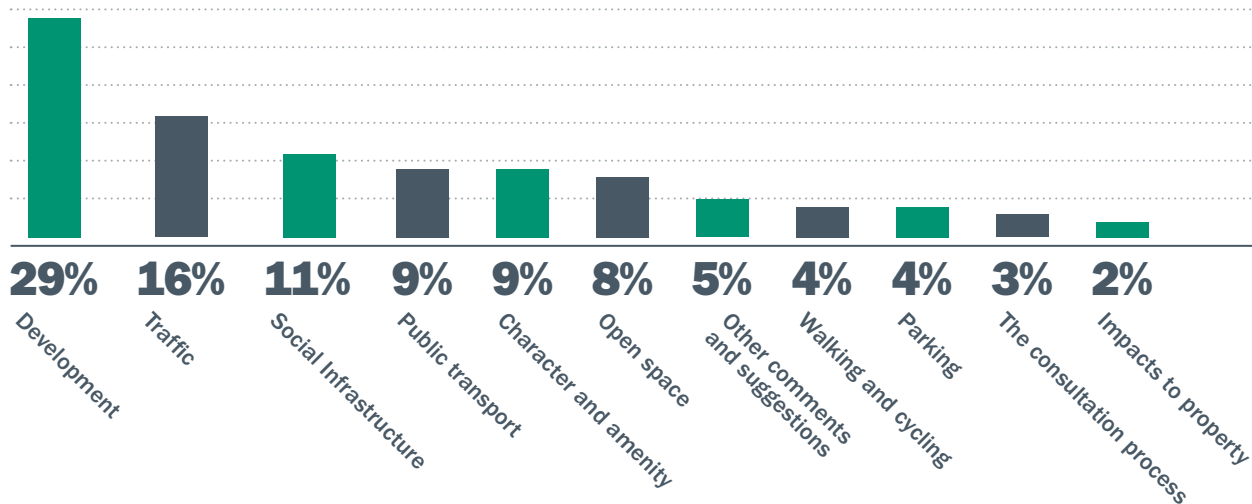
\* The total number of people who submitted a survey is greater than the number above, as not all participants linked their survey to a Precinct.

^ All activities were organised on an LGA basis.

 To see more details on the Kings Bay Precinct Plan please see the draft Parramatta Road Urban Transformation Strategy at [www.urbangrowth.nsw.gov.au](http://www.urbangrowth.nsw.gov.au)

## Feedback on key themes

The graph below presents council and community feedback on the Kings Bay Precinct Plan against 11 themes. This data is drawn from online/paper surveys and submissions. A breakdown of feedback from councils and the communities is provided in the following sections.



The percentages in the above graph indicate the frequency of comments made by key theme.

## What we heard from councils

Submissions on the Kings Bay Precinct Plan were made by the City of Canada Bay and Ashfield Councils. A summary of their comments and recommendations is provided below.

### City of Canada Bay

- State strongly that there are three fundamental issues that need to be resolved and that are critical to the implementation of the Strategy. They are:
  - transport
  - affordable housing, and
  - value capture
- Recommend the Local Environmental Plan (LEP) addresses floor space ratio (FSR) transfer option for owners whose properties are affected by proposed future connections
- Recommend the northern side of Kings Road and the western boundary of properties fronting Courland Street have a maximum height of three storeys as opposed to six to eight storeys. Alternatively, the boundary of the Kings Bay Precinct should be reduced to exclude properties on Courland Street
- Recommend the draft Strategy and any future planning instrument confirm that land will be acquired to deliver the proposed open space and vehicular connections
- Request guidelines are provided to specify the minimum acoustic standards for future developments
- Request the prioritisation of an integrated transport system with effective linkages between bus, rail and ferry services
- Recommend the proposed maximums for off street car parking and other car share initiatives be implemented to reduce car ownership and traffic generation
- Request the transport report be amended to remove reference to the right turn bay at the intersection of Regatta Road and Queens Road as it does not exist
- Request widening the roadway and upgrading the intersection of William Street and Parramatta Road be considered
- Request upgrading the intersections at Spencer and Taylor streets or terminating the connection at Walker Street be considered
- Request the intersection treatment be considered where Spencer Street intersects with Queens Road

- Request funding for the proposed replacement of the existing roundabout with traffic signals at the intersection of Lyons Road West and William Street
- Request the long term use of Barnwell Park should be investigated as alternative open space
- Request the funding for Charles Heath Reserve be redirected to the provision of a synthetic surface within the Concord Community and Recreation Precinct
- Request the prioritising of the construction of all the GreenWay missing links including under Parramatta Road and Longport Street
- Request that GreenWay landscape enhancement be included in the Open Space and Infrastructure Schedule 'Potential Active Transport Improvement' work list. The schedule should also include the creation, maintenance and protection of a continuous bushcare corridor and bikeway/pathway from Ashfield Aquatic Centre to Iron Cove, adjacent to Dobroyd Canal

### Ashfield Council

- Request the retaining of residential R2 Low Density land zonings along Dalmar Street between Scott Street and Lang Street
- Request B6 Enterprise Zone between Scott Street and Land Street should remain as is in the Ashfield LEP 2013, with exceptions relating to mixed use development for business and employment
- Request that Council remain in control of planning decisions concerning land use and development standards in the Ashfield LEP 2013
- Recommend the proposed dwelling densities take into consideration roadways, vehicular access and servicing, wide footpath verges and any open space areas
- Recommend calculations for floor space ratios and maximum building heights be demonstrated relative to site layout
- Request an Enterprise Zone for the provision of employment, business and local services
- Request the current Ashfield LEP strategy for land planning along Parramatta Road be maintained
- Recommend permanent affordable housing is provided and managed by an accredited affordable housing cooperative
- Concern about the impacts of the 40,000 additional dwellings along Parramatta Road in relation to traffic volumes, use and impacts on local streets
- Recommend the Strategy proposes measures to assess the local street road network for impacts from the development
- Recommend the implementation of local area traffic management to provide for pedestrian safety, cyclist safety and resident amenity, street trees and useable open space in local streets
- Request that GreenWay landscape enhancement be added to the Open Space Provision and Embellishment work list including pathways between Marion Street and Longport Street
- Request the Government Architect's Office cost estimates to construct the missing links of the GreenWay be allocated to the Urban Amenity Improvement Program
- Request technical details for green setbacks showing wide public footpath areas which accommodate space for tall tree planting, ground cover and water sensitive urban design. The locations for these areas must be maximised and accounted for in the relevant drawings
- Request a map that shows where upper level apartment building setbacks are required for resident amenity in line with NSW Development near Rail Corridors and Busy Roads – Interim Guidelines for the areas to the east of Burwood along Parramatta Road
- Request substantial additions to open and recreational space be planned and implemented
- Request UrbanGrowth NSW explain how it will use the feedback from community consultation and what further processes it will use to incorporate residents' feedback in determining appropriate land uses

## What we heard from the community

The summary of community feedback on the Kings Bay Precinct Plan below is presented in order of the themes that received the most comments in the online/paper survey and submissions (as shown in the graph on page 52). This summary reflects the most frequently raised topics under each theme – it is not exhaustive of all comments made on the Kings Bay Precinct Plan.

### Development

Development was the most frequently discussed theme in the feedback, accounting for 29% of comments made in surveys and submissions.

**The majority of feedback opposed the proposed heights for the Precinct, with many people considering 25 storeys as too high for the area, particularly in areas adjacent to single storey residential dwellings.**

Other comments included:

- concern about the impact of new development on existing residential areas
- request new developments be well designed.
- request building heights do not exceed 8-12 storeys
- support for higher density development around train stations
- interest in more affordable housing being provided in the area
- suggestions that the draft Strategy include more commercial, retail and business areas, and
- interest in new developments providing additional affordable housing.

📌 6 to 8 storey buildings will also block out the light for the entire street until around noon in the summer and 2pm in winter which is highly inappropriate. I really have no problems with the rest of the plan... Development is long overdue in the area’.

– *community feedback*

📌 Please don't allow developers to go to 14-25 stories. While 25 stories works in Burwood, it would not suit [the] Kings Bay [and] Canada Bay Area. It would overwhelm the rest of the area’.

– *community feedback*

### Traffic

Traffic accounted for 16% of comments made in surveys and submissions, with most people raising **concerns about increased population compounding existing congestion, particularly on Queens Road, Lyons Road, Bayview Road, Regatta Road, and Walker Street.** Other comments included:

- concerns WestConnex would not reduce traffic along Parramatta Road, and
- requests for more detailed traffic studies and road improvements.

### Social infrastructure

Social infrastructure accounted for 11% of comments made in surveys and submissions. Participants said there was a **need for more schools, day care, aged services, and health services** to support population growth.

### Public transport

Public transport accounted for 9% of comments made in surveys and submissions. The majority of feedback focused on the **need for improvements to public transport** to support for the projected population increases.



## Character and amenity

Character and amenity accounted for 9% of comments made in surveys and submissions. **The majority of feedback highlighted concerns about the potential for future developments to have negative impact on the local character and heritage.** Other comments included:

- support for Urban Amenity Improvement Program, with additional suggestions including a community BBQ area, swimming facilities, and more street landscaping.

‘I walk everywhere within the Five Dock area, so properly maintained foot paths are essential to me. At the moment the paths are very close to the constant flow of traffic. Wider paths or some barrier between the traffic and the pedestrian would be nice such as grass verges and trees’.

– *community feedback*

## Open space

Open space accounted for 8% of comments made in surveys and submissions, with **most feedback requesting more open space for the Precinct.** Other comments included:

- opposition to the proposed open space on Dalmar and Scott streets
- suggestions for Barnwell Park to be investigated as alternative open space, and
- suggestions to prioritise the construction of the GreenWay’s missing links.

## Other

Feedback that could not be accommodated into the other listed themes accounted for 5% of comments made in surveys and submission. Comments included:

- concern for loss of local business, and
- concern for low job growth projection.

## Walking and cycling

Walking and cycling accounted for 4% of comments made in surveys and submissions, with most people commenting on the need to improve pedestrian and cyclist pathways with a focus on safety.

‘I walk everywhere within the Five Dock area, so properly maintained foot paths are essential to me. At the moment the paths are very close to the constant flow of traffic. Wider paths or some barrier between the traffic and the pedestrian would be nice such as grass verges and trees’.

– *community feedback*

## Parking

Parking accounted for 4% of comments made in surveys and submissions. Most of the participants said there was existing parking shortages in the area and a need for more parking to accommodate population growth, particularly on William Street.

## Consultation process

Feedback on the consultation program accounted for 3% of comments made in surveys and submissions. Comments centered around people's concerns that feedback provided in the 2014 consultations was not reflected in the draft Strategy.

## Impacts to property

Impacts to property accounted for 2% of comments made in surveys and submissions. Most comments were from landowners regarding compulsory acquisitions and reductions in local property values.

## Our initial response

Further community consultation will occur prior to any proposed changes to local zoning and development controls. This consultation will occur in line with statutory processes. Where appropriate, key components of the Strategy will be included within the District Plans presently being developed by Greater Sydney Commission (GSC), Department of Planning and Environment (DPE) and councils. The District Plans will then be implemented through councils' Local Environmental Plans (LEPs) in coordination with the delivery of key infrastructure.

Below we have provided our initial responses to the matters most frequently raised in the feedback and outline how we will finalise the Strategy in collaboration with local councils and government agencies.

### Development

- We have engaged a consultant team to review and refine the land uses, scale and access and movement principles, including the area south of Parramatta Road.
- We will consider the economic impacts of replacing existing industrially zoned land with other land uses, including any consequential impact on the Five Dock town centre.
- Urban design consultants will review and test the way in which future development relates to existing buildings along Kings Road, and transition arrangements generally between the Kings Bay Precinct and existing adjoining developments.
- The draft urban design guidelines are being reviewed and updated. More detailed controls are being developed to address design excellence, landscaping, privacy, solar access, and protecting the existing character and amenity in appropriate locations.

### Affordable housing

- We are working closely with government agencies such as DPE and the Department of Premier and Cabinet (DPC) to finalise the NSW Government's position on housing diversity and affordability issues to ensure the Strategy is consistent with broader government policy.

### Public and active transport and traffic

- All council and community feedback about transport services will be provided to Transport for NSW (TfNSW) and Roads and Maritime Services (RMS) to inform their public transport and roads planning along the Corridor.
- Rezoning of land will be triggered by mechanisms that align the coordination and delivery of services and infrastructure as identified by DPE and GSC.
- We will more clearly identify initiatives that will support renewal of the Kings Bay Precinct, including improved local transport services, better cycling and walking routes, potential road improvements and intersection upgrades (e.g. Lyons Road, Queens Road, Lang Street and William Street), and potential changes to speed limits. We are keen to improve the way these initiatives are articulated in the final Strategy and Implementation Tool Kit.
- The local amenity improvement works proposed as part of the Urban Amenity Improvement Program (UAIP) will be finalised. The suggested active transport links identified in council and community feedback will be assessed against the established UAIP criteria, and in accordance with the process flowchart identified within the UAIP.



### Social infrastructure and open space

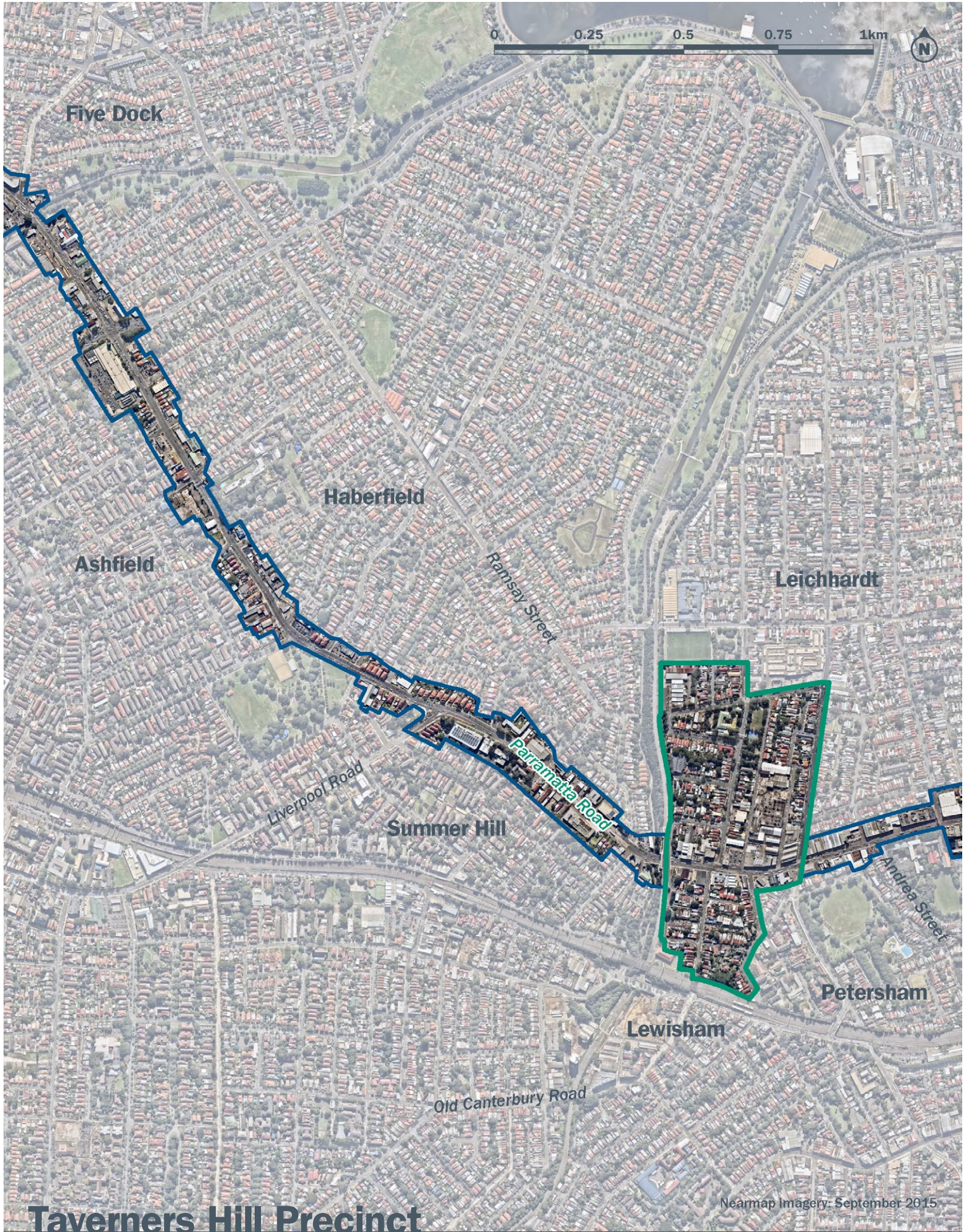
- The UAIP and the Open Space and Social Infrastructure Schedule will be updated to clearly articulate governance, funding, timing and any potential triggers or requirements that need to be satisfied.
  - We are working with the Government Architect's Office and other specialist consultants to confirm the optimal location, amount and type of future open space and social infrastructure required to support the renewal of the Kings Bay Precinct. This work will examine the proposed rates of provision identified and the need to provide passive and active recreational facilities.
  - We will also continue to work closely with the Department of Education (DOE), NSW Health and the Local Health District (LHD) to ensure schools and health facilities that can support the future population are planned properly.
- 



### Impacts to property

- Further community consultation will occur prior to any proposed changes to local zoning and development controls. These changes will be facilitated and approved by DPE, GSC or the local councils. Rezoning will only occur after the adoption of the final Strategy and is intended to be implemented in stages in response to the delivery of key infrastructure.
  - In the event that a compulsory land acquisition is needed to deliver infrastructure or open space, this will occur in full consultation with the landowner. UrbanGrowth NSW does not have the power to make compulsory acquisitions. Any land acquisitions would be undertaken by the state or local government agency responsible for delivering the infrastructure or open space.
  - We strongly recommend that landowners obtain independent professional advice prior to making any significant decisions about their property.
-

# TAVERNERS HILL



Nearmap Imagery: September 2015

 Precinct Area     Frame Area









# Feedback on the Taverners Hill Precinct Plan

**Vision:** Taverners Hill will be a urban village with walking and cycling links via the GreenWay, access to many public transport modes and many neighbourhood parks, squares and leafy streets.

**Source** [draft Parramatta Road Urban Transformation Strategy](#), September 2015. All vision statements were developed with councils.

## Engagement snapshot for the Taverners Hill Precinct

The following table outlines the diverse range of feedback channels and consultation activities^ that were organised for the Taverners Hill Precinct.

ACTIVITIES				
	<b>2</b> shopping centre displays	<b>1</b> community market pop up	<b>1</b> train station pop up	<b>1</b> community information and feedback sessions
RESPONSE CHANNELS				
	<b>84*</b> responses to the paper and online surveys	<b>7</b> focus group participants	<b>304</b> community submissions	<b>2</b> council submissions

\* The total number of people who submitted a survey is greater than the number above, as not all participants linked their survey to a Precinct.

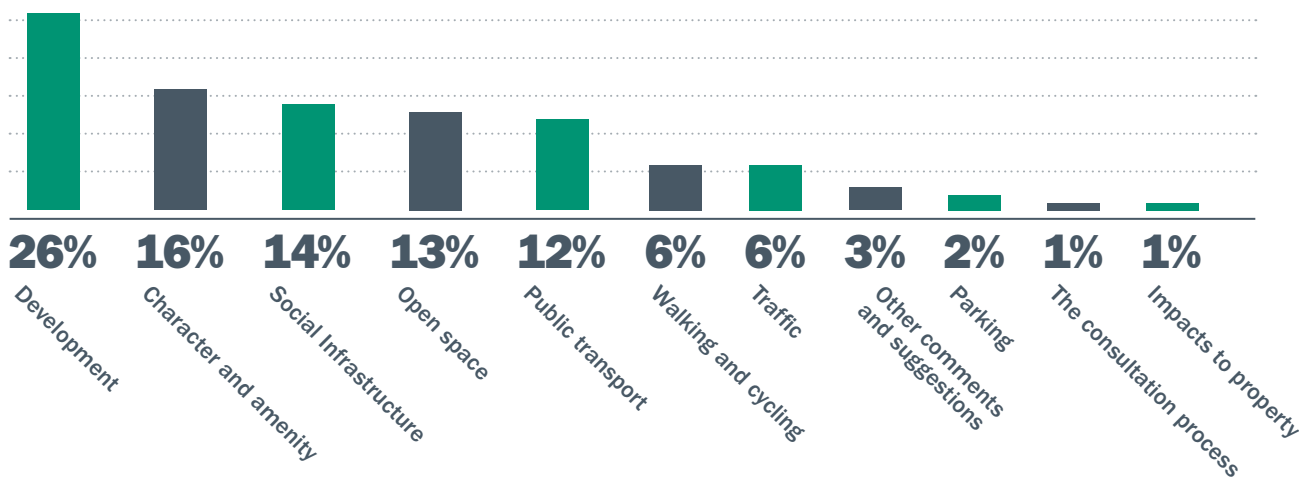
^ All activities were organised on an LGA basis.



To see more details on the Taverners Hill Precinct Plan please see the draft Parramatta Road Urban Transformation Strategy at [www.urbangrowth.nsw.gov.au](http://www.urbangrowth.nsw.gov.au)

## Feedback on key themes

The graph below presents council and community feedback on the Taverner Hill Plan against 11 themes. This data is drawn from online/paper surveys and submissions. A breakdown of feedback from councils and the communities is provided in the following sections.



The percentages in the above graph indicate the frequency of comments made by key theme.

## What we heard from councils

Council submissions on the Taverners Hill Precinct Plan were made by Marrickville Council and Leichhardt Municipal Council. A summary of their comments and recommendations is provided below.

In addition to their comments below on the draft Strategy, Marrickville Council proposed an alternative proposal for the Taverners Hill Precinct that focused on commercial and industrial sites. It aimed to provide improvements to active transport connections and open space, as well as reduced impact on existing dwellings. The summary below only relates to their comments on the draft Strategy and Taverners Hill Precinct Plan.

Similarly, Leichhardt Council submitted a preliminary alternative proposal detailing their vision for the Taverner’s Hill Precinct, which addressed industrial land, mixed uses, parks and a main spine on Flood Street. The summary below is of their comments on the draft Strategy and Taverners Hill Precinct Plan.

### Marrickville Council

- Recommend improvements to urban amenity, pedestrian connectivity and safety be facilitated by a reconfiguration of the road network and public domain
- Support opportunities identified in the Precinct Transport Report, noting that these commitments require statutory controls
- Request a range of additional improvements for Old Canterbury Road and Cook, Barker, Brown, Tebbutt and Hathern streets
- Recommend that public and active transport connections in the Precinct be more accessible, particularly to areas west of Old Canterbury Road. Other general improvements to the network are encouraged, including the widening of footpaths, street landscaping and, where required, on-street parking
- Welcome funding for the GreenWay missing links through the Urban Amenity Improvement Program
- Regard the proposed Cook Street open space location as unfeasible, suggesting Brown Street as an alternative location for open space
- Suggest the preservation of heritage and potential heritage items in this location is workable with three to four storeys

- Recommend a reduced scale for the Lewisham Hotel and neighbouring sites in order to retain the hotel and minimise the impact on dwellings to the south
- Support the staged implementation of the draft Strategy, with rezoning beginning in areas closest to transport and centres
- Recommend the Strategy form a State policy providing strategic guidance over a Section 117 Direction
- Support lower car parking rates, which are shown to have a positive effect on housing affordability. Car parking controls should be committed to in the statutory planning controls
- Note the Parramatta Road’s transformation was entirely dependent on reducing and better managing cars – achieved by limiting the traffic lane capacity and slowing it down
- Support the proposed heights and densities across the Precinct, but require more detail for each site
- Suggest innovative statutory controls be implemented to ensure development outcomes work with fine grain subdivision and have minimal impact on existing landholders
- Allow narrow frontage buildings and avoid lot amalgamation, supported by Precinct wide parking and servicing
- Rectify practical issues with some of the proposed built form, particularly in relation to overshadowing and sites with a heritage interface. These issues must be properly borne out in detailed, block by block master planning
- Consider treatment of heritage items with relation to adjoining development sites, to avoid potential reductions in amenity and value as single dwellings
- Recommend commitments to rapid transit and active transport to ensure the viability of investment in jobs and housing and to promote residential amenity

**Leichhardt Municipal Council**

- Recommend the IN2 industrial zoning is retained
- Suggest further investigations into the proposed scale of buildings on Parramatta Road, Tebbutt Street, and George Street
- Recommend the proposed pedestrian and public domain spine be relocated from Tebbutt Street to Flood and Thomas streets
- Suggest new through-site connections and vehicular connections, including an east west connection
- Concern about setbacks on Parramatta Road, site coverage, and the proposed super stop on Parramatta Road at Thomas Street
- Concern about open space at Kegworth Primary School
- Concern about change from mixed use to residential zoning
- Concern about the location of landmark buildings
- Interest in industrial lands supporting the local population and greater subregion
- Recommend retaining industrial zoning, or similar employment for Camperdown and Taverners Hill Precincts
- Support the increase in the number of jobs and dwellings in the Precincts
- Recommend refining the draft Strategy with detailed site testing and consideration of landownership and heritage
- Recommend maintaining mixed use character in the Local Government Area

## What we heard from the community

The summary of community feedback on the Taverners Hill Precinct Plan below is presented in order of the themes that received the most comments in the online/paper survey and submissions (as shown in the graph on page 60). This summary reflects the most frequently raised topics under each theme – it is not exhaustive of all comments made on the Taverners Hill Precinct Plan.

### Development

Development was the most frequently discussed theme in the feedback, accounting for 26% of comments made in surveys and submissions.

**The majority of feedback opposed the proposed heights and density of development in the Precinct, particularly in areas adjacent to Kegworth Primary School and properties south of Parramatta Road.**

Other comments included:

- support for density and redevelopment along Parramatta Road and in areas in close proximity to public transport
- suggestion that four-six storey buildings is a more appropriate maximum height for the area
- requests for scaling of buildings east of Tebbutt Street, and
- support for mixed use development near Lewisham Station.

📌 Density, infrastructure and liveability cannot be separated as they are part of the same equation. Wherever there is excessive density with insufficient infrastructure it results in poor liveability for residents’.

– *community feedback*

📌 Renew Parramatta Road first, it must be the priority and then [it can be] seen what could be developed around the area’.

– *community feedback*

### Character and amenity

Character and amenity accounted for 16% of comments made in surveys and submissions. **The majority of feedback highlighted concerns about the potential for future development to have negative impact on the local character and heritage, particularly the Lewisham Hotel and Thomas Street.** Other comments included suggestions for public domain improvements, such as landscaping and streetscaping.

📌 It is important that new development is well integrated with the existing area and community so that it represents growth of the area, not taking it over with a different form and character’.

– *community feedback*

### Social infrastructure

Social infrastructure accounted for 14% of comments made in surveys and submissions. Participants said there was **need for more social infrastructure** to support population growth, such as schools, child care and health services.

### Open space

Open space accounted for 13% of comments made in surveys and submissions, with most feedback requesting **more open space for the Taverners Hill Precinct.** Many comments supported **upgrades to the GreenWay.**



## Public transport

Public transport accounted for 12% of comments made in surveys and submissions. The majority of feedback focused on the **need for improvements to public transport** to support for the projected population increases.

‘Before meaningful urban design master planning can take place, commitments to the location and types of public transport systems along the corridor must be made. High level analysis and modelling of the implications of higher densities and the capacity of Parramatta Road to sustain traffic volumes and compatible modes of public transport must be undertaken as a priority. This is crucial to the feasibility and liveability of the proposed precincts and greater strategy’.

– *community feedback*

‘I'm not opposed to increasing building height, as long as there is transport infrastructure to support the residents/businesses for the increased density’.

– *community feedback*

## Walking and cycling

Walking and cycling accounted for 6% of comments made in surveys and submissions, with most people welcoming improvements to pedestrian and cyclist pathways. Many of the suggestions were to improve walking and cycling, particularly to improving their safety, widened footpaths, and intersection upgrades.

## Traffic

Traffic accounted for 6% of comments made in surveys and submissions, with most people raising concerns about increased population compounding existing congestion, particularly on Old Canterbury Road, Lords Road, Railway Terrace, and Tebbutt, Upward, Flood and West streets.

## Other

Feedback that could not be accommodated into the other listed themes accounted for 3% of comments made in surveys and submissions. Comments included:

- concerns about the impact of the proposed Precinct changes to the local endangered Bandicoot population
- requests for further detailed/technical information, and
- questions about the timeline, staging and funding of the Strategy.

## Parking

Parking accounted for 2% of comments made in surveys and submissions. Most of the comments said there were existing parking shortages in the area and there was need for more parking to accommodate population growth.

## Consultation process

Feedback on the consultation program accounted for 1% of comments made in surveys and submissions. The majority of feedback was from people requesting more detail on the Precinct Plan and further community consultation.

## Impacts to property

Impacts to property accounted for 1% of comments made in surveys and submissions, with comments from landowners regarding potential land acquisitions and decreases to property values.

## Our initial response

Further community consultation will occur prior to any proposed changes to local zoning and development controls. This consultation will occur in line with statutory processes. Where appropriate, key components of the Strategy will be included within the District Plans presently being developed by Greater Sydney Commission (GSC), Department of Planning and Environment (DPE) and councils. The District Plans will then be implemented through councils' Local Environmental Plans (LEPs) in coordination with the delivery of key infrastructure.

Below we have provided our initial responses to the matters most frequently raised in the feedback and outline how we will finalise the Strategy in collaboration with local councils and government agencies.



### Development

- We will consider elements of the alternate proposals put forward by Marrickville Council and Leichhardt Municipal Council in the refinement of the final Strategy.
- The proposed heights for the Taverners Hill Precinct will be reinvestigated. We have engaged a consultant team which will review and refine the land uses, scale and access and movement principles. This will consider if renewal should be limited to the immediate Parramatta Road frontage and industrial sites to the north of Parramatta Road. The impacts of that approach will be weighed up against the broader strategic planning issues facing Sydney, and recognising the Government's commitment to provide new and additional public transport in the Corridor.
- The team will also review and refine heights, particularly around sensitive areas such as Thomas Street, the Lewisham Hotel and Kegworth Public School. More detailed land use and built form controls are also being developed to address the issues of setbacks, heritage preservation, privacy and solar access, and character and amenity in appropriate locations.



### Public and active transport and traffic

- All council and community feedback about transport services will be provided to Transport for NSW (TfNSW) and Roads and Maritime Services (RMS) to inform their public transport and roads planning along the Corridor.
- Rezoning of land will be triggered by mechanisms that align the coordination and delivery of services and infrastructure as identified by DPE and GSC.
- We will continue to work closely with TfNSW to identify the potential for future upgrades to Lewisham Station and the light rail stops.
- The function and hierarchy of the local street network will be investigated in more detail. Traffic congestion on Old Canterbury Road, Tebbutt Street, Upward Street, Lords Road and Flood and West Streets will be analysed in more detail by traffic consultants.
- The implications of WestConnex, urban renewal in the Precinct and the proposed increase in population on traffic generation and movements will be investigated in consultation with TfNSW, RMS, DPE and the local councils.
- Car parking demand and supply will be investigated by our traffic consultant to respond to feedback from the community and Marrickville Councils. Planning controls and other mechanisms to promote less car parking and pedestrian friendly streets (in the right locations) will be developed for the final Strategy.

- We are keen to maximise opportunities to improve active transport links and streetscapes throughout the Precinct. We are committed to defining a clear, unambiguous and high quality network of pedestrian and cycling connections that show local and regional context, and clearly illustrate strategic opportunities to deliver direct connections, complete missing links and contribute towards completion of the GreenWay.



### Social infrastructure and open space

- The Urban Amenity Improvement Program and the Open Space and Social Infrastructure Schedule will be updated to clearly articulate funding, timing and responsibility for these improvements.
- In response to feedback received from both councils and the community, we will work with the Government Architect's Office and other specialist consultants to confirm the optimal location, amount and type of future open space and social infrastructure required to support the renewal of the Taverners Hill Precinct. This work will examine the proposed rates of provision, and the need to provide passive and active recreational facilities.
- We will also continue to work closely with the Department of Education (DOE), NSW Health and the Local Health District (LHD) to ensure schools and health facilities that can support the future population are planned properly.



### Impacts to property

- Further community consultation will occur prior to any proposed changes to local zoning and development controls. These changes will be facilitated and approved by DPE, GSC or the local councils. Rezoning will only occur after the adoption of the final Strategy and is intended to be implemented in stages in response to the delivery of key infrastructure.
- In the event that a compulsory land acquisition is needed to deliver infrastructure or open space, this will occur in full consultation with the landowner. UrbanGrowth NSW does not have the power to make compulsory acquisitions. Any land acquisitions would be undertaken by the state or local government agency responsible for delivering the infrastructure or open space.
- We strongly recommend that landowners obtain independent professional advice prior to making any significant decisions about their property.



## Leichhardt Precinct

 Precinct Area     Frame Area

# Feedback on the Leichhardt Precinct Plan

**Vision:** Leichhardt Precinct will be a vibrant mixed use entertainment precinct visited by people from all over Sydney, with retail and residential opportunities creating a rejuvenated and active Norton Street and Parramatta Road.

Source [draft Parramatta Road Urban Transformation Strategy](#), September 2015. All vision statements were developed with councils.


## Engagement snapshot for the Leichhardt Precinct

The following table outlines the diverse range of feedback channels and consultation activities<sup>^</sup> that were organised for the Leichhardt Precinct.

ACTIVITIES			
	<b>1</b> shopping centre display	<b>1</b> community market pop up	<b>2</b> community information and feedback sessions
RESPONSE CHANNELS			
	<b>47*</b> responses to the paper and online survey	<b>6</b> focus group participants	<b>203</b> community submissions
			
			<b>2</b> council submissions

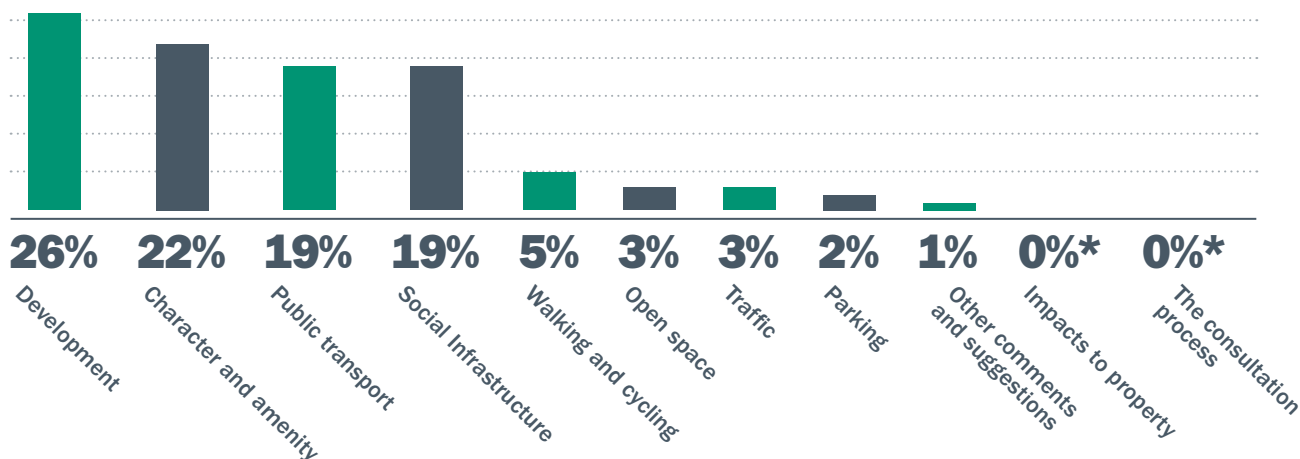
\* The total number of people who submitted a survey is greater than the number above, as not all participants linked their survey to a Precinct.

<sup>^</sup> All activities were organised on an LGA basis.

 To see more details on the Leichhardt Precinct Plan please see the draft Parramatta Road Urban Transformation Strategy at [www.urbangrowth.nsw.gov.au](http://www.urbangrowth.nsw.gov.au)

## Feedback on key themes

The graph below presents council and community feedback on the Leichhardt Precinct Plan against 11 themes. This data is drawn from online/paper surveys and submissions. A breakdown of feedback from councils and the communities is provided in the following sections.



The percentages in the above graph indicate the frequency of comments made by key theme.  
 \* represents less than 0.5% of responses

## What we heard from councils

Submissions on the Leichhardt Precinct Plan were made by the Leichhardt Municipal Council and Marrickville Council. A summary of their comments and recommendations is provided below.

In addition to the comments summarised below, Leichhardt Council submitted a preliminary alternate Structure Plan, extending along Parramatta Road and Norton Street. The councils' vision for the area focused on revitalising Norton Street through intensification of uses, improving permeability, public domain upgrades and retaining the built form character of the Precinct.

### Leichhardt Municipal Council

- Request for more open space in the Precinct
- Concern the scale of proposed buildings is not sensitive to the existing streetscape
- Request the consideration of the Conservation Area and heritage items, through relevant detailed site testing
- Suggestion for streets adjacent to Parramatta Road to form public domain spaces through setbacks to development
- Strong support for the proposed extension of McDonald Street and Dot Lane, with suggestions for more opportunities for through-site connections to Norton Street
- Suggestion for active frontages along Parramatta Road and Norton Street allowing for non-residential ground level uses along the length of Parramatta Road and Norton Street serving as the primary retail and public domain area
- Request revisions to controls, particularly for site coverage
- Recommend the location of the public transport Super Stop be changed

### **Marrickville Council**

- Support for lower car parking rates, which are shown to have a positive effect on housing affordability, with car parking controls in the statutory planning controls
- Recommend the Strategy form a state policy providing strategic guidance over a Section 117 Direction
- Request the rapid transit interchange be relocated to avoid demolition in the heritage conservation areas on both sides of Parramatta Road
- Request treatment of heritage items be considered with relation to adjoining development sites to avoid potential reductions in amenity and value of single dwellings
- Request documentation to support immediate action on housing affordability, with the mandatory inclusion of affordable housing and value uplift capture in the Strategy
- Support proposed heights and densities across the Precinct, however, more detail is sought for each site
- Recommend innovative statutory controls be implemented to ensure development outcomes work with fine grain subdivision and have minimal impact on existing landholders
- Request a solution to allow narrow frontage buildings and avoid lot amalgamation, supported by Precinct-wide parking and servicing
- Concern about the interface of proposed development with heritage sites, particularly overshadowing

## What we heard from the community

The summary of community feedback on the Leichhardt Precinct Plan below is presented in order of the themes that received the most comments in the online/paper survey and submissions (as shown in the graph on page 68). This summary reflects the most frequently raised topics under each theme – it is not exhaustive of all comments made on the Leichhardt Precinct Plan.

### Development

Development was the most frequently discussed theme in the feedback, accounting for 26% of comments made in surveys and submissions. **The majority of feedback opposed the proposed heights and density of development in the Precinct, with many people considering 25 storeys too high for the area, particularly in areas adjacent to existing residential dwellings and local heritage.**

Other comments included:

- support for development along Parramatta Road
- support of the proposed extensions of McDonald Street and Dot Lane, with further opportunities for through-site connections to Norton Street
- support for stepping down building heights around sensitive areas
- questions about the accuracy of the housing projections for the Precinct
- support for more affordable housing in the Precinct, and
- concerns about the conflict between Parramatta Road as a throughway and increasing the density and liveability of the area.

📌 We need sensible controls that provide a framework that gives the confidence to invest and renew’.

– *community feedback*

### Character and amenity

Character and amenity accounted for 22% of comments made in surveys and submissions. **The majority of feedback highlighted concerns about the proposed developments’ negative impact on the local character and heritage.** The Urban Amenity Improvement Program was well supported with the community suggesting to incorporate public art into the program. Other comments included:

- suggestions for more community areas and street trees/greenery.

📌 Whilst the pocket park opposite Norton Street is a good gesture, more meaningful parks in the Precinct should be explored. There are very [few] public space/squares in Leichhardt Town Centre to sit’.

– *community feedback*

📌 Urban amenity improvements to the character of Parramatta Road could include street plantings between parking. One lane of Parramatta Road could be allocated to bus stops, parking and street planting allowing a greater buffer between the vehicle ‘corridor’ and the pedestrian and shopfront activity’.

– *community feedback*

### Public transport

Public transport accounted for 19% of comments made in surveys and submissions. The majority of feedback focused on the **need for improvements to public transport** to support the projected population increases.



## Social infrastructure

Social infrastructure accounted for 19% of comments made in surveys and submissions. Participants said there was **need for more social infrastructure** to support population growth, such as schools, day care, aged services, and health services.

## Walking and cycling

Walking and cycling accounted for 5% of comments made in surveys and submissions, with most people commenting on the **need to improve pedestrian and cyclist pathways** with a focus on safety.

## Open space

Open space accounted for 3% of comments made in surveys and submissions, with most feedback requesting more open space for the Precinct and the Leichhardt local government area generally.

## Traffic

Traffic accounted for 3% of comments made in surveys and submissions, with most people raising concerns about increased population compounding existing congestion. Some suggestions were made for improving the intersections of Norton Street and Parramatta Road, and Crystal Street and Parramatta Road.

📌 [It's] great to have more vibrant and attractive public spaces. There doesn't seem to be much about managing traffic, infrastructure, schools, aged facilities and other community services. More dense population will mean more cars. There is already a gridlock on weekends getting in and out of Norton Street'.  
– *community feedback*

## Parking

Parking accounted for 2% of comments made in surveys and submissions. Most people commented that there were existing parking shortages in the area and the need for more parking to accommodate population growth, particularly on William Street.

## Other

Feedback that could not be accommodated into the other listed themes accounted for 1% of comments made in surveys and submissions.

## Impacts to property

Impacts to property accounted for less than 0.5% of comments made in surveys and submissions. Most comments were from landowners regarding compulsory acquisitions and reductions in local property values.

## Consultation process

Feedback on the consultation program accounted for less than 0.5% of comments made in surveys and submissions. The majority of feedback was from people requesting more detail on the Precinct Plan and further community consultation.

## Our initial response

Further community consultation will occur prior to any proposed changes to local zoning and development controls. This consultation will occur in line with statutory processes. Where appropriate, key components of the Strategy will be included within the District Plans presently being developed by GSC, DPE and councils. The District Plans will then be implemented through councils' Local Environmental Plans (LEPs) in coordination with the delivery of key infrastructure.

Below we have provided our initial responses to the matters most frequently raised in the feedback and outline how we will finalise the Strategy in collaboration with local councils and government agencies.



### Development

- We will seek to review the draft Strategy, Precinct Plans and Urban Design Guidelines to strengthen the principles and controls to ensure that active and vibrant uses are attracted, local businesses are viable and a mix of uses and services are provided.
- The stretch of Parramatta Road between Hawthorn Canal and Broadway is considered iconic based on its urban fabric, built form and fine grained subdivision pattern. The proposed heights in this section of Parramatta Road and the Leichhardt Precinct are being informed by further development of the 'fine grain study' that was previously exhibited.
- The significance of the built form fabric, subdivision and road pattern is being analysed in order to:
  - identify properties that have the potential to undergo minor redevelopment or more ambitious urban transformation
  - develop principles and controls to sensitively repair, restore and refine existing fabric
  - identify compatible new, long-term uses that could allow buildings to be retained despite significant changes in its surrounding environment, and
  - recommend planning controls to guide the sympathetic renewal of this stretch of the Corridor.
- We have engaged a heritage consultant and architect. Working with our urban designer and the recommendations of the fine grain study, the consultant team will:
  - map all existing heritage items and heritage conservation areas across the entire Corridor, including the Leichhardt Precinct and Frame Area, for inclusion in the final Strategy, and
  - review and refine heritage controls for inclusion in the Urban Design Guidelines, with particular regard to heritage floor space transfers, minimum setbacks, integration and transition principles.



### Public and active transport and traffic

- All council and community feedback about transport services will be provided to Transport for NSW (TfNSW) and Roads and Maritime Services (RMS) to inform their public transport and roads planning along the Corridor.
- Rezoning of land will be triggered by mechanisms that align the coordination and delivery of services and infrastructure as identified by Department of Planning and Environment (DPE) and Greater Sydney Commission (GSC).
- The function and hierarchy of the local street network will be investigated in more detail. The implications of WestConnex, urban renewal in the Precinct and the cumulative impacts of other urban renewal projects is also being investigated in consultation with TfNSW, RMS, DPE and the local councils.

- Car parking demand and supply is being investigated by our traffic consultant including requirements to support businesses along Norton Street.
- We are keen to optimise better active transport links and streetscapes throughout the Precinct. The pedestrian and cycling connections and opportunities to deliver direct connections, and complete missing links will be examined.



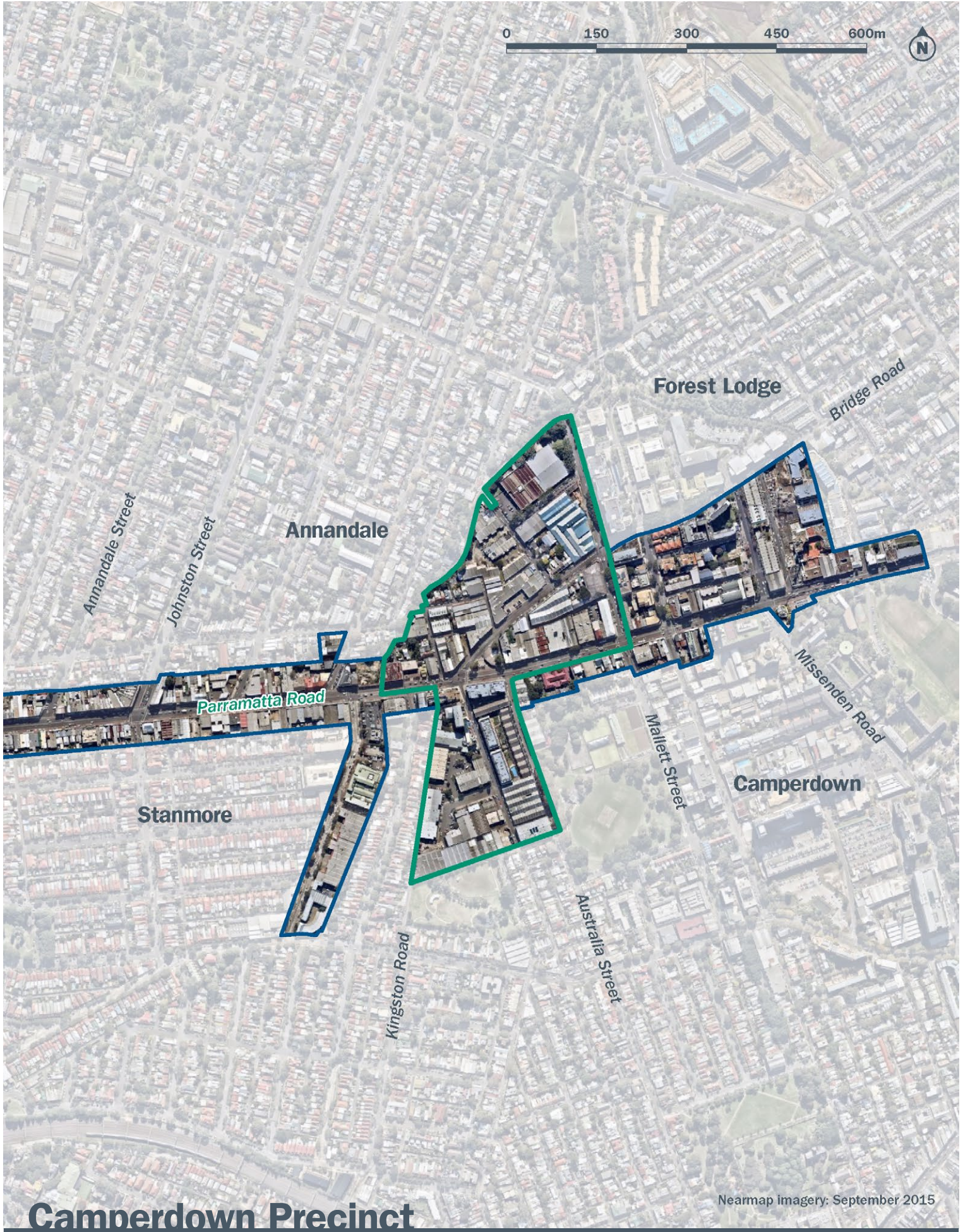
### Social infrastructure and open space

- We are working with the Government Architect's Office and other specialist consultants to confirm the optimal location, amount and type of future open space and social infrastructure required to support the renewal of the Precinct. This work will examine the proposed rates of provision identified in the draft Strategy, and the need to provide passive and active recreational facilities.
- We will also continue to work closely with the Department of Education (DOE), NSW Health and the Local Health District (LHD) to ensure schools and health facilities that can support the future population are planned properly.
- The Urban Amenity Improvement Program (UAIP) and the Open Space and Social Infrastructure Schedule will be updated with the scope, funding and delivery arrangements for these improvements.



### Impacts to property

- Further community consultation will occur prior to any proposed changes to local zoning and development controls. These changes will be facilitated and approved by DPE, GSC or the local councils. Rezoning will only occur after the adoption of the final Strategy and is intended to be implemented in stages in response to the delivery of key infrastructure.
- In the event that a compulsory land acquisition is needed to deliver infrastructure or open space, this will occur in full consultation with the landowner. UrbanGrowth NSW does not have the power to make compulsory acquisitions. Any land acquisitions would be undertaken by the state or local government agency responsible for delivering the infrastructure or open space.
- We strongly recommend that landowners obtain independent professional advice prior to making any significant decisions about their property.



## Camperdown Precinct

 Precinct Area  Frame Area








# Feedback on the Camperdown Precinct Plan

**Vision:** Camperdown Precinct will be home to high-quality housing and workplaces right on the edge of the CBD, well connected to the surrounding city, parklands, health and education facilities and focused on a busy and active local centre.

Source [draft Parramatta Road Urban Transformation Strategy](#), September 2015. All vision statements were developed with councils.

## Engagement snapshot for the Camperdown Precinct

The following table outlines the diverse range of feedback channels and consultation activities<sup>^</sup> that were organised for the Camperdown Precinct.

ACTIVITIES	 <b>1</b> shopping centre display	 <b>1</b> community market pop up	 <b>1</b> train station pop up	 <b>1</b> community information and feedback sessions
	RESPONSE CHANNELS	 <b>79*</b> responses to the paper and online survey	 <b>8</b> focus group participants	 <b>230</b> community submissions

\* The total number of people who submitted a survey is greater than the number above, as not all participants linked their survey to a Precinct.

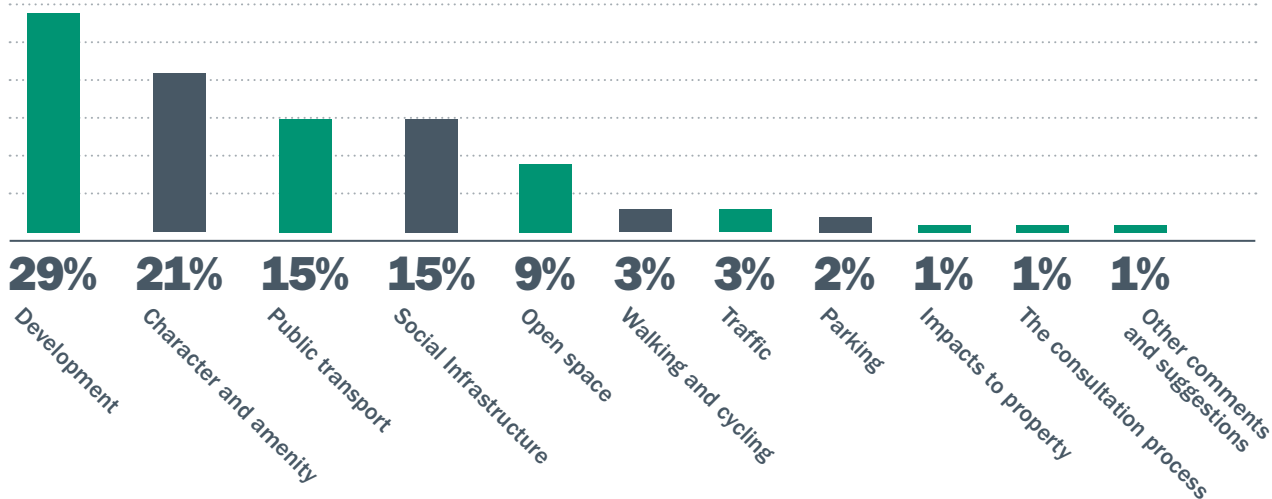
<sup>^</sup> All activities were organised on an LGA basis.



To see more details on the Camperdown Precinct Plan please see the draft Parramatta Road Urban Transformation Strategy at [www.urbangrowth.nsw.gov.au](http://www.urbangrowth.nsw.gov.au)

## Feedback on key themes

The graph below presents council and community feedback on the Camperdown Precinct Plan against 11 themes. This data is drawn from online/paper surveys and submissions. A breakdown of feedback from councils and the communities is provided in the following sections.



The percentages in the above graph indicate the frequency of comments made by key theme.

## What we heard from councils

Submissions on the Camperdown Precinct Plan were made by the City of Sydney Council, Leichhardt Municipal Council and Marrickville Council. A summary of their comments and recommendations is provided below.

### The City of Sydney

- Concern that WestConnex will worsen congestion with the proposed portal east of Missenden Road
- Note that finding an effective public transport solution for the area is an ongoing challenge
- Note that the Strategy will need to respond to the NSW Government's housing affordability policy once it is completed
- Request more detail about the planning and funding of open space and community services
- Note that volatility in the housing market will influence the timing and delivery of growth
- Note that the draft Strategy does not meet the open space benchmarks set in the NSW Department of Planning and Environment guidelines
- Concern that noise and pollution on Parramatta Road make it unsuitable for residential development as shown in the Department of Planning's Apartment Design Guide (SEPP 65 Objective 4J-1)
- Concern about the limited capacity of Parramatta Road to provide additional public transport
- Concern about the impact of the proposed development on Camperdown's character and amenity
- Recommend targets be developed with councils for open space, recreation facilities and community facilities
- Recommend the development of an appropriate infrastructure funding and delivery strategy
- Recommend a value capture or sharing mechanism be used to maximise the opportunities presented by the government's investment and by the increases in land value from rezoning for higher densities
- Recommend that Precincts outside a ten minute walk to an existing rail station not proceed until a suitable high capacity public transport solution for Parramatta Road is funded
- Recommend that the draft Strategy ensures the retention of Camperdown's heritage listed sites

### Leichhardt Municipal Council

- Recommend the proposed heights be reduced to be more sensitive to the local character
- Recommend that current IN2 industrial zoning be retained
- Support the retention of employment land
- Oppose the mixed use zonings
- Recommend the heritage, fine grain industrial built form character is retained
- Recommend Pyrmont Bridge Road and Chester Street be the focus of streetscape improvements and active uses to promote their role as main streets
- Recommend the intersection of Parramatta Road and Booth Street/Mallet Street be considered for a transport super stop
- Recommend the fine grain street and lane network be retained and enhanced
- Recommend improvements to pedestrian connections to existing parks, destinations and infrastructure
- Recommend improvements to east-west active transport connections
- Suggest the removal of the proposed vehicle connection through the heritage building between Chester Street and Booth Street
- Suggest the creation of a continuous open space connection between Parramatta Road and Booth Street on the western boundary of the precinct

### Marrickville Council

- Recommend the strip to the west of the new north-south street be reduced to average three or four storeys, with adequate space between Cardigan Lane and a substantial tree buffer
- Recommended the area bounded by Kilner Lane, Denison Street, Cardigan Lane and the new street link be reduced to average six to eight storeys to integrate retention of the Franks building at 2 Cardigan Lane
- Recommend the preservation of Cardigan Street Heritage Conservation Area
- Recommend using Johnston Creek water course and Cardigan Street/Lane linkages as a significant green corridor for active transport, enhanced greening and biodiversity
- Recommend the replacement of proposed open space opposite O'Dea Reserve with an extension of the proposed vehicular connection parallel to Cardigan Lane
- Support the expanding employment opportunities in the education and health sectors

## What we heard from the community

The summary of community feedback on the Camperdown Precinct Plan below is presented in order of the themes that received the most comments in the online/paper survey and submissions (as shown in the graph on page 76). This summary describes the most frequently raised topics under each theme – it is not exhaustive of all comments made on the Camperdown Precinct Plan.

### Development

Development was the most frequently discussed theme in the feedback, accounting for 29% of comments made in surveys and submissions.

**The majority of feedback opposed the proposed heights and density of development in the Precinct, particularly along Susan Street.**

Other comments included:

- suggestions that increased height and density would be more appropriate along Pyrmont Bridge Road and Parramatta Road
- recommendations to reduce the average height in the area bounded by Kilner Lane, Denison Street, Cardigan Lane and the new street to eight storeys, with the view to retain the Franks Building
- concerns the proposed setbacks and building envelopes are insufficient for the proposed increases in density and height, noting that proposed envelopes will reduce visibility and natural light
- concerns the proposed land uses for the Precinct may restrict local employment options, and
- support for more affordable housing in the proposed developments.

📌 We are happy to support a new development and residential conversion from commercial/ industrial zoning to share great amenities and city proximity, however at commensurate scale to existing adjacent areas and not compromise quality of life of new and existing residents’.

– *community feedback*

📌 Appropriate setbacks and building envelopes will significantly reduce the visual impact and loss of natural light and amenity to the existing low rise dwellings in Susan Street and surrounds’.

– *community feedback*

### Character and amenity

Character and amenity accounted for 21% of comments made in surveys and submissions. The majority of feedback highlighted concerns about the **potential for future development to have a negative impact on the local character and heritage**, especially along Cardigan Street and in Union Square.

📌 If residential development is to go ahead we believe there needs to be sensitive treatment of the interface between the existing residential heritage precinct and new development. This should include green space along Cardigan Lane and ideally a sustainable transport corridor. There needs to be plantings along this zone to protect the privacy of residents who back on to Cardigan Lane. This will assist with localised flooding and create a habitat corridor which will allow native animals to move across the LGA’.

– *community feedback*

📌 If the intention is to create a residential area between Cardigan Lane and Australia Street, then it is highly inappropriate to make Australia Street a major road... Turning this into a major road will cut off the community from the park. An alternative could be Cardigan Street or Bridge Road to the west. The community atmosphere and easy pedestrian access to the park make it a major hub’.

– *community feedback*



## Public transport

Public transport accounted for 15% of comments made in surveys and submissions. **The majority of feedback focused on the need for improvements to public transport to support the projected population increases.** An emphasis was placed on these improvements being made prior to the proposed development.

📌 The public transport initiatives ... are all subject to funding. Given there are no guarantees about any improvements, our concern is that [a] public transport solution will be an after-thought with Transport for NSW waiting until it becomes a problem rather than forward planning’.

– *community feedback*

## Social infrastructure

Social infrastructure accounted for 15% of comments made in surveys and submissions. **Participants said there was need for more social infrastructure to support population growth, such as schools and health services.** Many comments said there was an existing shortage of social infrastructure in the area.

## Open space

Open space accounted for 9% of comments made in surveys and submissions, with **most feedback requesting more open space for the Precinct.** Other comments included:

- concerns that existing open space will be overshadowed by increased development
- concerns the locations of new proposed open space is inappropriate for the area
- suggestion for the creation of a continuous open space connection between Parramatta Road and Booth Street on the western boundary of the Precinct
- suggestion for creation of north-south, and greenlink along Johnstons Creek and along Cardigan Lane.

## Walking and cycling

Walking and cycling accounted for 3% of comments made in surveys and submissions. Most comments focused on the need to improve pedestrian and cyclist pathways by making routes more direct and safer.

📌 Denison Street could easily become an active transport designated street (perhaps local traffic only; or low speed shared use pedestrian/ bike/ cars), providing a strong access route between Parramatta Road and Bridge Road’.

– *community feedback*

## Traffic

Traffic accounted for 3% of comments made in surveys and submissions, with most people raising concerns about increased population compounding existing congestion.

## Parking

Parking accounted for 2% of comments made in surveys and submissions. Most of the comments said there is an existing parking shortage in the area and a need for more parking to accommodate population growth.

## Impacts to property

Impacts to property accounted for 1% of comments made in surveys and submissions. Most comments were from landowners regarding compulsory acquisitions.



## Consultation process

Feedback on the consultation program accounted for 1% of comments made in surveys and submissions. The majority of feedback was from people requesting further detail and additional consultation with local residents about the proposed changes.

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## Other

Feedback that could not be accommodated into the above themes accounted for 1% of comments made in surveys and submissions. Comments included:

- concerns about flood zones in the Precinct, and
  - support for expanding employment opportunities in the area's education and health sectors.
-

## Our initial response

Further community consultation will occur prior to any proposed changes to local zoning and development controls. This consultation will occur in line with statutory processes. Where appropriate, key components of the Strategy will be included within the District Plans presently being developed by Greater Sydney Commission (GSC), Department of Planning and Environment (DPE) and councils. The District Plans will then be implemented through councils' Local Environmental Plans (LEPs) in coordination with the delivery of key infrastructure.

Below we have provided our initial responses to the matters most frequently raised in the feedback and outline how we will finalise the Strategy in collaboration with local councils and government agencies.



### Development

- We have engaged a consultant team to review and refine the land uses, scale and access and movement principles.
- In response to feedback from Leichhardt Council and the Sydney Local Area Health District (SLHD), we are re-investigating whether existing industrially zoned land in the Camperdown Precinct should be used for long term employment, health or education purposes.
- We agree that the future Camperdown Precinct should respond to its context and marker buildings such as the Franks Building. The way in which future development relates to existing buildings along Susan Street, Denison Street, Kilner Lane, Cardigan Street and Cardigan Lane is also important. The proposed heights and densities are therefore being reviewed and tested by an urban design consultant, as are the proposed draft urban design guidelines in respect of buildings along these streets. More detailed land use and built form controls are being developed for setbacks, heritage preservation, privacy and solar access, and protecting the character and amenity.



### Public and active transport and traffic

- All council and community feedback about transport services will be provided to Transport for NSW (TfNSW) and Roads and Maritime Services (RMS) to inform their public transport and roads planning along the Corridor.
- Rezoning of land will be triggered by mechanisms that align the coordination and delivery of services and infrastructure as identified by DPE and GSC.
- We will also more clearly identify the initiatives that are required to support renewal of the Camperdown Precinct, including improved local transport services, better cycling and walking routes, intersection upgrades, road improvements, and potential changes to speed limits.
- North-south crossings on Parramatta Road will be analysed in more detail as the planning for the Camperdown Precinct takes shape and the preferred public transport mode is considered by TfNSW.
- In response to concerns about car parking demand, we have commissioned a survey to better understand where and how parking is used, particularly around existing or planned transport nodes. The results from this survey will inform planning controls and other measures to reduce car ownership and promote the use of public transport.
- We will undertake further traffic analysis to examine the proposed renewal of Camperdown in the context of Westconnex and other urban renewal projects such as the Bays Precinct and Central to Eveleigh will inform the final Strategy.



## Social infrastructure and open space

- We are working with the Government Architect's Office and other specialist consultants to confirm the optimal location, amount and type of future open space and social infrastructure required to support Camperdown.
  - We are developing concepts for a new north-south green link along Johnston Creek from Wigram Road to Parramatta Road and then south along Cardigan Lane to incorporate into the draft Strategy.
  - We will also continue to work closely with the Department of Education (DOE), NSW Health and SLHD to ensure schools and health facilities that can support the future Camperdown population are planned for.
  - The funding and delivery timeframes will be addressed in the final draft Strategy.
- 



## Impacts to property

- Further community consultation will occur prior to any proposed changes to local zoning and development controls. These changes will be facilitated and approved by DPE, GSC or the local councils. Rezoning will only occur after the adoption of the final Strategy and is intended to be implemented in stages in response to the delivery of key infrastructure.
  - In the event that a compulsory land acquisition is needed to deliver infrastructure or open space, this will occur in full consultation with the landowner. UrbanGrowth NSW does not have the power to make compulsory acquisitions. Any land acquisitions would be undertaken by the state or local government agency responsible for delivering the infrastructure or open space.
  - We strongly recommend that landowners obtain independent professional advice prior to making any significant decisions about their property.
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# 4

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## Community engagement methodology

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# Our approach to consultation

The consultation program was designed to reach the diverse communities with an interest in the Corridor’s future. By understanding the views and needs of these diverse groups, we can deliver better long-term outcomes for the Corridor.

In collaboration with the 10 local councils, we developed an extensive suite of engagement techniques to achieve the following consultation objectives:

- draw on knowledge and experience from stakeholders and the local community along the corridor to better inform project planning
- identify and manage community concerns and feedback
- identify and increase awareness about the program and the wider benefits of urban renewal
- keep the community and all stakeholders informed about the progress of the program

- identify and mitigate the program risks
- build positive relationships with stakeholders and the community, and
- continually review and improve the engagement strategy based on lessons learned.

The consultation program drew on best practice principles in line with the approach advocated by the International Association for Public Participation.

For a full list of the techniques used for communicating and collecting feedback, please see the table below and the Appendices.

## How we reached the community

The table below provides a description and rationale for the different engagement techniques used during the consultation period. A calendar of these events and activities is provided in Appendix D.

Engagement technique	Description	Why we did it
<b>Community information and feedback sessions</b>	<p>10 sessions were held outside of business hours at accessible venues, in each of the Corridor’s local government areas</p> <p>Project information was on display, fact sheets and draft strategies were available to take home and subject matter experts were available to answer questions</p> <p>Also, team members recorded feedback and were available to support people doing surveys</p> <p>Interpreters were present to translate information into other languages</p>	<ul style="list-style-type: none"> <li>▪ Provide a face-to-face opportunity for the community to learn about the draft Strategy, to ask questions of the project team, and to provide feedback</li> <li>▪ Build relationships between community stakeholders and the project team</li> </ul>
<b>Online/paper survey</b>	<p>A survey with questions about key elements of the draft Strategy and Precinct Plans was available online, at community information sessions and at pop-up stalls</p>	<ul style="list-style-type: none"> <li>▪ Seek qualitative input and feedback on key elements of the draft Strategy</li> <li>▪ Allow the community to tell us what was important to them</li> </ul>

Engagement technique	Description	Why we did it
<b>Written submissions</b>	<p>People were invited to write to us on the draft Strategy about any matters that were important to them</p> <p>Submissions could be sent via email, written in the free text box on the website, mailed in hard copy or given to project staff at community information sessions and pop-up stalls</p>	<ul style="list-style-type: none"> <li>Seek qualitative input and feedback on key elements of the draft Strategy</li> <li>Allow the community to tell us what was important to them</li> </ul>
<b>Random telephone survey</b>	<p>Interviews with 518 residents from across the Corridor</p> <p>Participants were provided with material to review ahead of the interviews</p>	<ul style="list-style-type: none"> <li>Provide statistically significant and demographically representative feedback on the draft Strategy</li> </ul>
<b>Dedicated project website</b>	<p>Provided a wide range of project information including the draft Strategy, news, publications, fact sheets, the consultation calendar, video updates and survey</p>	<ul style="list-style-type: none"> <li>Provide a central access point for the community to get information and give feedback on the draft Strategy, including those who live outside the area</li> </ul>
<b>Newsletter</b>	<p>Newsletter with information about the draft Strategy and details of consultation events was delivered to 105,000 homes in and near the Corridor. This included translator information for non-English speaking residents</p> <p>The newsletter was also translated into ten languages and made available at local councils, and distributed to cultural peak bodies/associations and available at events</p>	<ul style="list-style-type: none"> <li>Raise awareness of the draft Strategy amongst people who are potentially interested and impacted, including people from non-English speaking backgrounds</li> </ul>
<b>Pop up stalls</b>	<p>23 pop up stalls at shopping centres, train stations, festivals and markets across the Corridor, with project staff to provide information, answer questions and distribute newsletters and documents in both English and other languages</p>	<ul style="list-style-type: none"> <li>Raise awareness of the draft Strategy and consultation opportunities amongst community members</li> <li>Increase the number and diversity of community voices giving feedback</li> </ul>
<b>Focus groups</b>	<p>10 focus groups with people who live in and near the Corridor, including representatives of hard-to-reach groups such as youth and culturally and linguistically diverse communities</p>	<ul style="list-style-type: none"> <li>Seek in-depth community discussion and feedback on each of the Precinct Plans</li> </ul>

Engagement technique	Description	Why we did it
<b>Newspaper advertisements</b>	19 advertisements were placed in local newspapers inviting community members to consultation events across the Corridor. These included 12 advertisements in non-English publications and seven advertisements in English language publications	<ul style="list-style-type: none"> <li>Promote the draft Strategy and consultation activities across the Corridor</li> </ul>
<b>Facebook advertising</b>	Advertisements providing project and consultation information	<ul style="list-style-type: none"> <li>Boost awareness of the project and consultation activities, particularly amongst young people</li> </ul>
<b>Facebook posts on UrbanGrowth NSW and local councils' pages</b>	<p>Facebook posts were placed on the New Parramatta Rd Facebook page each week during the consultation period</p> <p>Content and images for Facebook posts were also provided to 10 local councils to promote the draft Strategy and events in their Local Government Area</p>	<ul style="list-style-type: none"> <li>Reach people who live in and near the corridor that prefer to receive information via online channels</li> <li>Reach community members via council networks to encourage participation during the consultation process</li> </ul>
<b>Letters to landowners</b>	Two letters each sent to 22,000 landowners in the Corridor with key information about the draft Strategy	<ul style="list-style-type: none"> <li>Inform landowners about the draft Strategy, and encourage participation in the consultation process, including those who are not residents of the Corridor</li> </ul>
<b>Landowner meetings</b>	15 meetings with landowners and developers across the Corridor	<ul style="list-style-type: none"> <li>Answer landowner questions about potential impacts to their property</li> </ul>
<b>Translation and interpreter services</b>	<p>The following services were used during the consultation period:</p> <ul style="list-style-type: none"> <li>the newsletter was translated into 10 languages</li> <li>a telephone translation service was provided, and</li> <li>interpreters attended every community information and feedback session</li> </ul>	<ul style="list-style-type: none"> <li>Ensure the consultation program was inclusive of culturally and linguistically diverse communities</li> </ul>
<b>University participation</b>	Lecture to university students about how large-scale urban transformation projects are planned and implemented	<ul style="list-style-type: none"> <li>Raise awareness of the draft Strategy and its rationale amongst university students</li> </ul>
<b>School participation</b>	Information about consultation and engagement activities and opportunities provided to schools to disseminate through student, parent and staff networks	<ul style="list-style-type: none"> <li>Raise awareness of the draft Strategy for students, teachers and parents</li> </ul>



## Interpreting the feedback

Feedback was collected using the following feedback channels:

- online/paper survey
- written submissions (qualitative data), and
- random telephone survey (quantitative data).

A description of how this feedback was interpreted is provided below.

### The online/paper survey and written submissions

We commissioned Elton Consulting to manage the online/paper survey and written submissions that were used to seek people's views and suggestions on the changes proposed to the whole of the Corridor and the individual Precincts. This qualitative method of capturing feedback allowed people to tell us what was important to them. All comments were examined individually and, where possible, the views and suggestions will be used to inform the refinement and finalisation of the draft Strategy.

These comments were also categorised into key themes through a process known as 'coding'. By coding the information we calculated the frequency that different topics were raised – this assisted us to understand a theme's relative importance and the strength of support/opposition to different aspects of the draft Strategy.

### The themes used for coding

The themes used for coding was based on the way information was presented in the draft Strategy. These themes were:

- development
- public transport
- traffic
- parking
- walking and cycling
- social infrastructure
- character and amenity
- open space
- impacts to property
- consultation process, and
- other issues and suggestions.

### How we coded the submissions

The feedback was coded using the following guidelines:

- Each submission was counted once and then coded according to the themes raised.
- For petitions, each signature was counted as an individual submission. That is, if 25 people signed a petition, the themes captured were multiplied by 25.
- A submission made on behalf of multiple people was counted according to the number of signatures that endorsed the submission.
- Multiple submissions from the one person were collated and coded as one submission.
- Where people sent duplicates of their submission via different channels, it was counted as one submission.
- In the instance where a community member asked a question, but did not make a comment, no coding was conducted. In these instances, where appropriate, community members were encouraged to provide a written submission or to complete a survey.

To see the graphs produced from the coded online/paper survey and submissions, please see chapter 3.

### The random telephone survey

We commissioned Jetty Research to conduct a random telephone survey so that we could complement the qualitative findings from the online/paper survey and submissions with data that was statistically representative of the views of communities along the Corridor.

The survey was conducted using a two-step process:

- 800 potential survey respondents from across the Corridor and surrounds were sent a package containing information about the draft Strategy.
- Each candidate was invited to participate in a phone interview. A total of 518 respondents completed the phone interview between 30 November and 8 December 2015.

### **About the telephone survey questions**

The survey was designed to be quick and easy, taking about approximately 15 minutes to complete in order to maximise participation.

The survey questions asked people about their views on the Corridor, what needed improving and their responses to key aspects of the draft Strategy. All questions related to the whole of the Corridor. We did not ask specific questions about the eight individual Precincts.

### **About the interviewees**

The survey was completed by 518 residents from the Parramatta Road Corridor and surrounds. The interviewees included people from a range of age groups, people who speak a language other than English at home, people who own or rent their home, and people from a mix of housing types. Over half of the interviewees live within a Precinct, with the remainder living close to a Precinct.

The results can be seen as statistically representative of the wider adult population in the Corridor and surrounds, to within approximately +/-4.3% random sampling error at the 95% confidence level.

For more detail on the methodology and results of the random phone survey please see Appendix B.



Wrapped Up \$13  
- Portuguese Spicy fish,  
Crunchy lettuce, tomato &  
aioli  
- Traditional Lamb kofta,  
Cabbage, tomato & aioli

I ❤️  
Bar Biscotti

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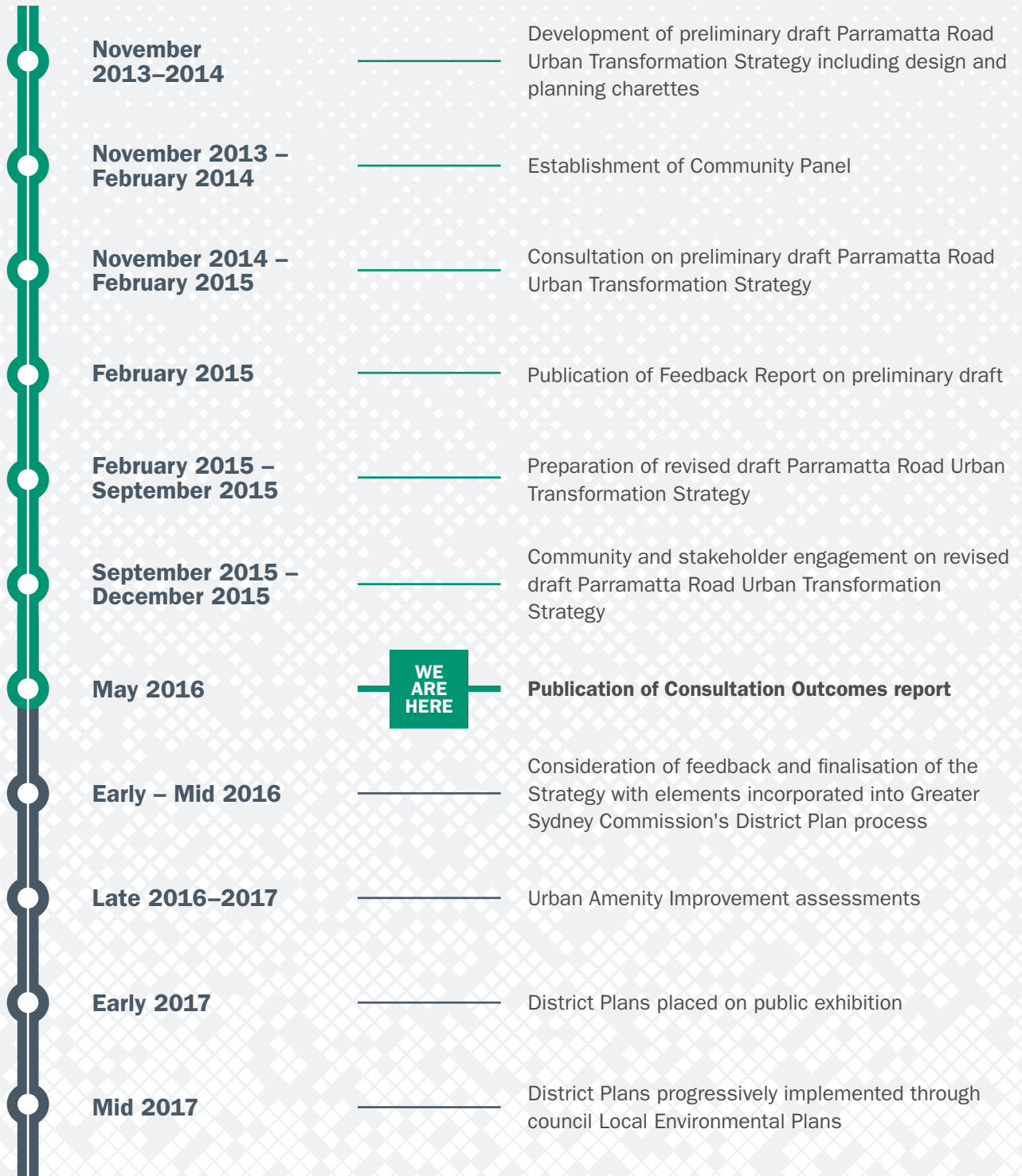
Next steps



## Finalising the strategy

Now that consultation on the draft Strategy is complete the project team is considering all the feedback received. The draft Strategy will be finalised and presented to Government for adoption in 2016.

You can keep up to date by visiting [www.urbangrowth.nsw.gov.au](http://www.urbangrowth.nsw.gov.au)



Please note timing is indicative





For further information about the  
Strategy, implementation plan  
and to register for updates:  
**[www.urbangrowth.nsw.gov.au](http://www.urbangrowth.nsw.gov.au)**



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